

DOWNTOWN SPECIFIC PLAN

CITY OF MARTINEZ, CALIFORNIA

**Adopted by
City Council
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Chapter 1 - Introduction

I Introduction

- 1.1 What is a Specific Plan?
- 1.2 Specific Plan Area
- 1.3 Purpose of the Downtown Specific Plan
- 1.4 How the Specific Plan works
- 1.5 Applicability and conformity with the Specific Plan
- 1.6 General Plan consistency
- 1.7 Zoning Ordinance consistency
- 1.8 Supporting Documents
 - Acknowledgements

1.1 What is a Specific Plan?

A Specific Plan is a tool authorized by the California Government Code Sections 65450 – 65457 for the systematic implementation of the General Plan in a defined portion of a community’s planning area. A Specific Plan must specify in detail the distribution, location and extent of land uses for the area; public and private facilities proposed to be located in the area and needed to support the land uses; standards and criteria by which development will proceed; standards for the conservation, development, and use of natural resources, where applicable; and a program of implementation measures, including financing measures.

1.2 Specific Plan Area

The Downtown Martinez Specific Plan study area covers about 220 acres and is bounded on the north by the Martinez Regional Shoreline and Martinez Waterfront Park; on the east by the Shell Martinez Refinery and a hillside residential area accessed from Miller Avenue; on the south by Susana Street; and on the west by cemeteries, Rankin Park, Talbart and Richardson Avenues, and by Thomas Hill, the bluff to the west of Berrellesa Street. (See Figure 1-1.) The study area includes the Downtown commercial and civic core and surrounding neighborhoods.

1.3 Purpose of the Downtown Specific Plan

The purpose of the Downtown Specific Plan is to guide public and private investment to ensure that future development and infrastructure projects help realize the Martinez community’s goals and visions for the future of Downtown. The principal goals are complementary and are meant to be mutually reinforcing:

1. To enhance the quality of life for Martinez residents, and
2. To bring back commercial dynamism to the downtown business area.

The Specific Plan is thus intended to protect and perpetuate the small-town character and quality of life of downtown Martinez; revitalize its economy by strengthening the

Downtown business district as a shopping and dining destination for Martinez residents and visitors; capitalize on past investments such as the waterfront, Alhambra Creek, and Intermodal Station; create new housing opportunities for a variety of household types; and preserve and enhance Downtown's historic small-town character.

Further, the Specific Plan is intended to promote "smart growth" and "sustainable development" in Downtown Martinez, by:

1. Providing for compact, pedestrian-oriented development.
2. Providing for denser housing within walking distance of transportation centers.
3. Taking advantage of existing infrastructure.
4. Providing for mixed land uses.
5. Requiring attractive, distinctive design for new development

1.4 How the Specific Plan Works

The Specific Plan is designed to establish a vision and development framework for the Downtown and the means to implement that vision. The Plan will be implemented through public investments and private development projects. Implementation will be regulated through detailed development standards, land use regulations, design standards and guidelines, and the City's design review process.

1.5 Applicability and Conformity with the Specific Plan

The provisions of this Specific Plan shall apply to all properties included in the Downtown Specific Plan area. No construction, substantial modification, demolition, addition, placement or installation of any building structure shall occur, nor shall any new use commence on any lot, on or after the effective date of this Specific Plan, except in conformity with the provisions of this Specific Plan.

1.6 General Plan Consistency

This Specific Plan is not a component of the Martinez General Plan. It is a separately adopted general plan implementation document. To ensure consistency between the Downtown Specific Plan and the Martinez General Plan, the General Plan map and text will be amended prior to the adoption of this Plan to be consistent with this Plan. In addition, the Central Martinez Specific Area Plan will be amended to include the Downtown Specific Plan land use designations to replace the Specific Area Plan designations for the Specific Plan area. This is consistent with General Plan section 10.1, which provides for the periodic review and amendment of the General Plan to reflect changes in conditions and values as they occur.

Outside of the Specific Plan area, the provisions of the "Central Martinez Specific Area Plan" section of the Martinez General Plan will continue to apply to the remainder of the Central Martinez area as designated in the General Plan.

1.7 Zoning Ordinance Consistency

To ensure consistency between the Downtown Specific Plan and the City of Martinez Zoning Ordinance, the Zoning Map will be amended concurrent with the adoption of this Plan to include three new zoning districts: Downtown Shoreline, Civic, and Historic Overlay. The Central Commercial zoning district will be amended for use in the Downtown Core area.

Where land use regulations and/or development standards of the Martinez Zoning Ordinance are inconsistent with this Specific Plan, the standards and regulations of the Specific Plan shall prevail. Any issue not specifically covered in the Specific Plan shall be subject to the Zoning Code and/or Municipal Code. Interpretations may be made by the Director of Community Development or referred to the Planning Commission if not specifically covered in the City's existing regulations.

1.8 Supporting Documents

Several documents were prepared as background information for the development of the Downtown Martinez Specific Plan. These supporting documents are available at Martinez City Hall and are as follows:

Downtown Martinez Economic and Demographic Survey and History
Prepared by Strategic Economics, June 2003

City of Martinez Existing Transportation Conditions
Prepared by Fehr & Peers Associates, July 2003

Summary of Workshop #1 Survey Results
Prepared by Calthorpe Associates, August 2003

Downtown Martinez Infrastructure and Public Facilities Assessment
Prepared by Bellecci & Associates, September 2003

Transit Options Research in Downtown Martinez
Prepared by Fehr & Peers Associates, September 2003

Downtown Martinez Opportunity Sites Memorandum
Prepared by Strategic Economics, September 2003

Downtown Martinez Parking Count Results
Prepared by Fehr & Peers Associates, October 2003

Downtown Martinez Retail Market Assessment
Prepared by Strategic Economics, November 2003

Downtown Martinez Employment, Office and Industrial Assessment
Prepared by Strategic Economics, November 2003

Summary of Alternative Development Concepts and Assessments
Prepared by Calthorpe Associates, February 2004

Economic Assessment of Alternative Development Concepts
Prepared by Strategic Economics, February 2004

Traffic Impacts of Economic Revitalization Alternative
Prepared by Fehr & Peers Associates, February 2004

Acknowledgements

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Contra Costa County Capital Facilities Dept.
Martinez Chamber of Commerce
Main Street Martinez
Old Town Preservation & Beautification
Committee



Fig 1-1 STUDY AREA

LEGEND

- - - Study Area Boundary
- ++++ Rail Road
- Property Lines
- ~ Alhambra Creek
- Parks / Open Space

Downtown Martinez Specific Plan

Martinez, California



0' 200' 400' 600' 800'



Berkeley, California
City of Martinez, California

Chapter 2 –Goals and Policies

The purpose of this chapter is to describe the issues identified for Downtown and guiding principles developed from community input, and to establish goals and policies for the Specific Plan based on these principles. This chapter is organized as follows:

2 Goals and Policies

2.1 Issues Summary

2.2 Goals and Policies

2.1 ISSUES SUMMARY

During the initial phases of the planning process, the following issues were identified:

- **Loss of Economic Vitality** – Martinez residents and policy makers expressed concern about the health of Downtown business and its ability to accommodate the needs of the community.
- **Vulnerable Building Stock** – Many Downtown commercial buildings are built of unreinforced masonry and would be damaged beyond repair in a major earthquake. In addition, many buildings are in dire need of physical reinvestment after decades of deferred maintenance. If new investment is not made in the physical structures Downtown, the costs of revitalizing these buildings will continue to increase until the only option remaining is demolition.
- **Lack of Consensus** – While most stakeholders agreed that economic development was desirable Downtown, opinions varied widely regarding the need for change in land use policies Downtown and the appropriateness of various implementation tools for achieving revitalization. By creating and analyzing a range of options, the Specific Plan process aimed to aid local decision makers in achieving consensus on a direction for Downtown land use regulations and policies.
- **Zoning and Development Standards** – Downtown’s zoning categories and related development standards were outdated and did not address important Downtown issues.
- **Historical/Cultural** – Existing zoning and development standards do not adequately address historic preservation issues.
- **Parking** – Downtown parking is in high demand in certain areas and at certain times, and visitors sometimes have trouble navigating their way to parking areas.
- **Character** – The need to preserve and respect the existing small-town character and sense of community in the older parts of Downtown Martinez.

2.2 GOALS AND POLICIES

The goals and policies in this section set forth the framework for realizing the Specific Plan guiding principles. In turn, the land use districts, development and design standards, and implementation strategies for Downtown establish the framework for evaluating development proposals, public improvements, and the implementation of action plans.

Where the standards and regulations of the Specific Plan do not directly address an issue, the goals, policies, and principles established herein, and those of the General Plan, shall be used to determine an appropriate course of action.

2.2.1 Land Use Goals and Policies

Goal LU-1: To provide land use opportunities for Downtown Martinez to serve as a cultural, arts and entertainment center offering a wide range of opportunities for residential lifestyles, work environments, shopping, entertainment, culture and the arts.

Policy LU-1-1: Maintain the integrity of each Downtown area as follows:

- **Downtown Core:** The mixed-use center of Downtown, with an emphasis on a pedestrian-scale mixture of residential, specialty commercial, tourist, restaurant, cultural and civic uses. In addition to rehabilitation and adaptive reuse of historic buildings, the development philosophy emphasizes new and infill construction that is compatible with the historic structures that give Downtown its unique identity.
- **Civic Area:** A higher intensity district primarily intended for civic, governmental and judicial uses, interspersed with supporting offices and commercial businesses. In addition to preservation and restoration of landmark governmental and civic buildings, the design philosophy encourages development with interpretive ties to Martinez' heritage architecture.
- **Downtown Historic Overlay District:** The historic commercial, civic and mixed-use streetscapes that form the heart of Downtown. The design philosophy is centered on historic preservation, rehabilitation and adaptive reuse of existing structures.
- **Downtown Neighborhood and Grandview Areas:** Predominantly residential areas surrounding the Downtown Core. The design philosophy is centered on historic preservation, sensitivity, and neighborhood compatibility.
- **Downtown Shoreline:** The developed area, currently in industrial use, between the railroad tracks and the ~~Martinez Regional Shoreline~~ Downtown Core and Neighborhood Districts. The land use strategy is centered on relocating industry and creating new development that is in keeping with the traditional Downtown character. ~~and contributes to the protection and enhancement of the existing open space and shoreline.~~ Note: strike out type wording not adopted. Reserved for possible future action or amendment.

Policy LU-1-2: Encourage pedestrian-oriented specialty retail shops offering quality goods and services in the Downtown Core and a range of restaurants that

includes fine dining, cafes, coffee houses, and sandwich shops. Encourage balance between individually owned business and franchise or corporate entities.

Policy LU-1-3: Encourage transit-oriented and mixed-use development with a strong residential presence in the Downtown Core, including both new construction and the adaptation of upstairs spaces in existing buildings for residential purposes.

Policy LU-1-4: Provide incentives for infill development throughout Downtown, with an emphasis on the opportunity sites identified in this Plan.

Policy LU-1-5: Encourage the establishment of a vibrant mix of uses that will serve the needs of both residents and visitors and will help create a vibrant daytime, evening and weekend environment.

Policy LU-1-6: Maintain a continuity of pedestrian activity through active retail and restaurant ground level uses along Main Street, Ferry Street, Estudillo Street north of Main Street, and Las Juntas Street, north of Ward Street.

Policy LU-1-7: Encourage the relocation of heavy or large-scale industrial uses away from the Downtown while retaining service commercial businesses that are compatible with other Downtown land uses.

Policy LU -1- 8: Promote investment in unreinforced masonry buildings, to bring them up to current standards of construction and utility service.

Policy LU – 1 – 9: Encourage construction of residential development within walking distance of the City’s Intermodal Station (Amtrak) to encourage use of rail passenger service.

2.2.2 Open Space Goals and Policies

Goal OS-1: To help attract visitors, residents and jobs to Downtown Martinez, maintain and improve the existing open space system in and around the Downtown and Shoreline.

~~Policy OS 1-1: Implement the standards in this Plan for any development adjacent to the Martinez Regional Shoreline to protect the ecological health of the Shoreline areas. NOTE: not adopted. Reserved for possible future action or amendment.~~

Policy OS-1-2: Implement the provisions of the Alhambra Creek Enhancement Plan to make a restored and improved creek a central attraction of Downtown.

2.2.3 Housing Goals and Policies

Goal H-1: To help Downtown Martinez succeed as an active daytime, evening and weekend downtown, encourage transit and pedestrian oriented housing in areas in addition to the traditional residential neighborhoods, to include the Downtown Core and areas currently in industrial use.

Policy H-1-1: Provide a variety of housing options affordable to varied income groups, including single-family houses, townhouses, live-work loft space, condominiums and apartments, and mixed-use buildings with a residential component.

Policy H-1-2: Preserve and enhance the residential neighborhoods in the Downtown.

Policy H-1-3: Provide incentives for ownership housing in the Downtown.

Policy H-1-4: Encourage adaptive reuse of existing structures, or the development of new buildings, for the purpose of live/work space in the Downtown Core.

Policy H-1-5: Encourage and promote new transit and pedestrian oriented residential projects, new secondary residential units, and the use of upstairs spaces in existing buildings in the Downtown Core for housing to increase housing options and help bring daytime, evening and weekend activity to the Downtown.

2.2.4 Economic Development Goals and Policies

Goal ED-1: Strengthen Downtown as a local and regional destination for specialty shopping, dining, nightlife, employment, culture and the arts.

Policy ED-1-1: Foster entrepreneurship, with an emphasis on entertainment, dining, culture, and resident-serving goods and services in the Downtown Core, and an emphasis on bed & breakfast inns and home occupations in the Downtown Neighborhoods.

Policy ED-1-2: Pursue an aggressive program to recruit quality commercial and office tenants and consider incentives to attract these uses.

Policy ED-1-3: Target key infill residential opportunities including small lot and row homes, townhouses, apartments and condominiums and live/work loft space.

Policy ED-1-4: Consider a financing plan that determines the scope of public improvements to be funded, the responsibilities of the public and private sector participants, and the methods of financing the improvements.

2.2.5 Urban Design Goals and Policies

Goal UD-1: Strengthen the identity and character of Downtown using the existing historic and architectural urban character of the community, while allowing for new structures that are architecturally compatible with, and complementary to, the existing architectural and historic fabric.

Policy UD-1-1: Through design review, ensure that new development enhances the character of the Downtown Districts by requiring design qualities and elements that contribute to an active pedestrian environment, where appropriate, and ensuring that architectural elements are compatible and in scale with the existing historic structures in the Downtown.

Policy UD-1-2: Enhance the connection between the Downtown and Waterfront by working with the East Bay Regional Park District to establish effective pedestrian and vehicular connections. These connections should include well-designed public art, lighting and landscaping as necessary to enhance the pedestrian environment.

Policy UD-1-3: Improve streetscapes on key corridors in the Downtown and create a sense of arrival at key gateways which reinforce the City's natural, cultural and historic characteristics.

Policy UD-1-4: Develop a funding program to implement the streetscape and gateway design concepts for the Downtown.

Policy UD-1-5: Encourage appropriate public art and interpretational signage to further establish a sense of history and pride in the Downtown.

2.2.6. Historic Preservation Goals and Policies

Goal HP-1: Strengthen and enhance the historic character of Downtown Martinez, which is unique to Contra Costa County, through the preservation and maintenance of Downtown's historically significant sites and structures.

Policy HP-1-1: Promote community appreciation for the history of Martinez.

Policy HP-1-2: Provide incentives to encourage the restoration of private historic structures to conserve the integrity of the buildings in the best possible condition.

Policy HP-1-3: Through design review, encourage new development to be compatible with adjacent historical structures in scale, massing, building materials, and general architectural treatment.

2.2.7. Circulation Goals and Policies

Goal C-1: Improve the circulation system in Downtown by maintaining and improving the grid system, providing for convenient access to, and circulation within, Downtown for all modes of transportation, and enhancing walkability in Downtown.

Policy C-1-1: Use the multi-modal street hierarchy for Downtown identified in this Plan to prioritize street improvement projects and target them to appropriate types of streets. Re-evaluate the use of one-way street segments within Downtown as part of a comprehensive Downtown circulation concept and parking strategy, with the objective of eliminating them except for major arteries into and out of town, such as Alhambra, Berrellesa, Escobar and Marina Vista..

Policy C-1-2: Provide enhanced transit amenities within the Downtown, including bus stops and shelters, transit information, and facilities at the Intermodal Station.

Policy C-1-3: Develop strong pedestrian connections between the Waterfront, the Intermodal Station, and the Downtown Core. Use streetscape improvements such as street trees, special paving, and ornamental streetlights to encourage pedestrian mobility at the sidewalk level.

Policy C-1-4: Do not permit any further street closures in the Downtown area, except for possible future conversion of streets to pedestrian malls.

Policy C-1-5: Provide continuous bike lanes on major streets approaching Downtown and within Downtown where warranted by traffic volumes.

Policy C-1-6: Improve way-finding signage, including: directional, destination-related, and signage for parking facilities as well as signs identifying regional trails.

2.2.8. Parking Goals and Policies

Goal P-1: Provide a balanced overall supply of parking for Downtown land uses, and make better and more effective use of the existing parking supply resources.

Policy P-1-1: Develop a coordinated, area-wide approach to parking in the Downtown that is based on sharing parking resources and a “park once” strategy. The overall approach should include convenient visitor parking in the central area of Downtown, in facilities designed to serve multiple uses, in order to ensure safe and efficient operation at all times of the day and evening.

Policy P-1-2: Develop employee parking strategies to avoid conflict with visitor parking.

Policy P-1-3: Work with the County and Court System to find parking solutions that address the needs of downtown businesses, County and Court employees, and jurors.

Policy P-1-4: Provide additional parking for the Intermodal Station to meet the needs of rail passengers.

Policy P-1-5: In order to encourage residential uses in the Downtown Core, develop an ordinance to allow payment of a fee towards construction of a parking structure, in lieu of providing the normally-required onsite parking spaces.

Chapter 3–Downtown Land Use Areas

This chapter establishes the Land Use Areas for the Downtown Specific Plan area and provides general provisions applicable to this Section. This chapter is organized as follows:

- 3 Downtown Land Use Areas
 - 3.1 Introduction
 - 3.2 Purpose and Applicability
 - 3.3 General Provisions

3.1 INTRODUCTION

Community goals for Downtown Martinez include economic development, neighborhood and historic preservation, creation of housing opportunities, and planning for the eventual relocation of industrial uses. (Please see Appendix C, Development of This Plan, for a discussion of the community process for this Plan.) The Land Use Areas in the Downtown Specific Plan are intended to support these goals by providing for an appropriate mix of uses and applying development and design standards that preserve and enhance the character of the Downtown.

Figure 3-1 illustrates the five Land Use Areas and one Overlay District for the Downtown Specific Plan area. These Areas are as follows:

- Downtown Core
- Civic
- Downtown Shoreline
- Downtown Neighborhood
- Grandview
- Downtown Historic Overlay District

3.2 PURPOSE AND APPLICABILITY

The development and design standards and guidelines for the Downtown Martinez Specific Plan area are intended to provide property owners, merchants and their designers with basic development and design criteria that are intended to reinforce the desired building and district character.

3.3 GENERAL PROVISIONS

3.3.1 List of Land Uses

This Plan does not contain a list of allowed land uses; those are found in the City’s Zoning Ordinance.

3.3.2 Standards Not Listed

Any issue or standard not specifically covered in this Plan shall be subject to the City of Martinez Zoning Code. In cases where development standards set forth in this Plan are inconsistent with the Zoning Code, the standards of the Plan shall prevail.

3.3.3 Interpretation

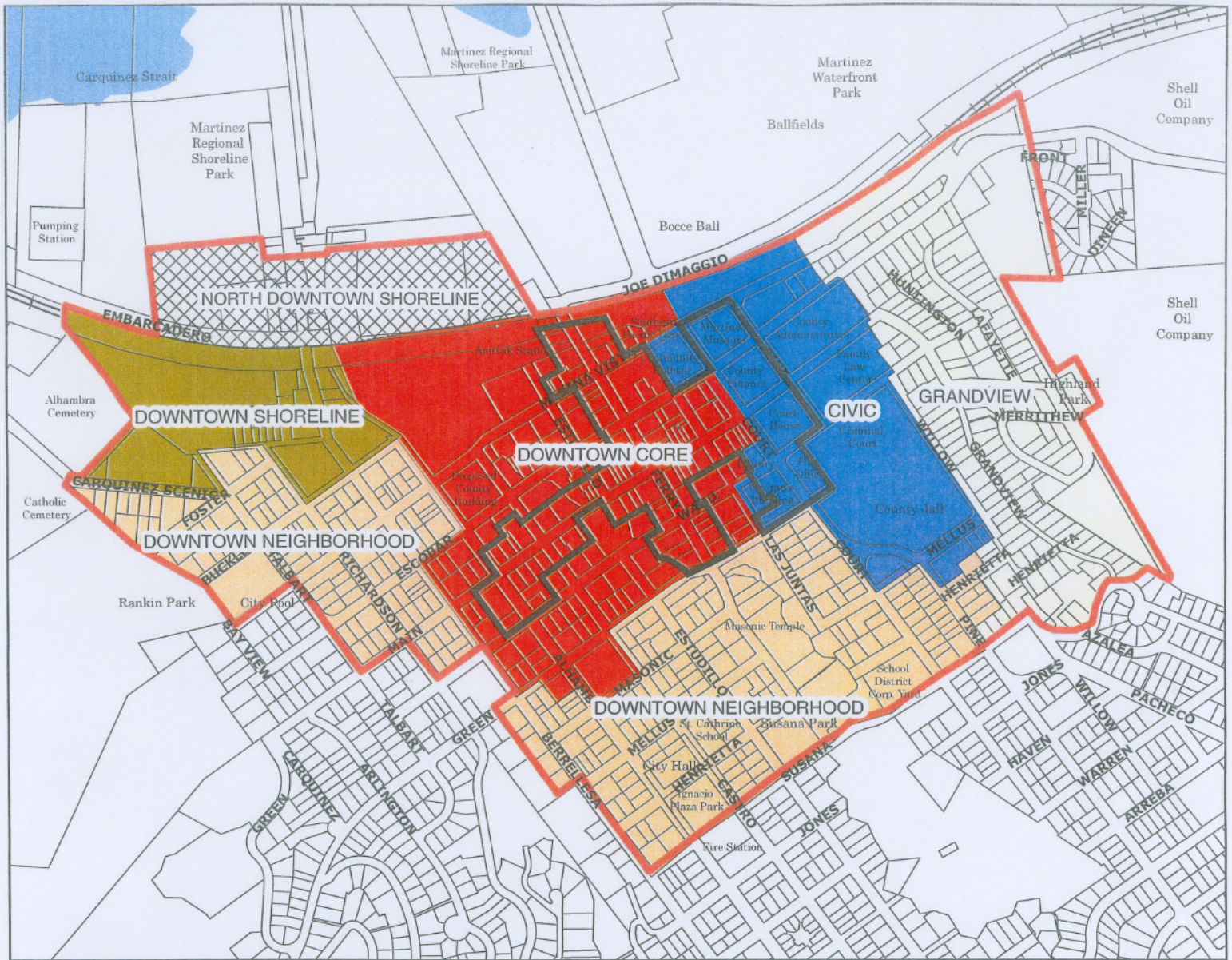
The design guidelines are general and may be interpreted by the Design Review Committee for specific projects with some flexibility, consistent with the purpose of the district. Variations may be considered for projects with special design characteristics during the City's design review process to encourage the highest level of design quality while at the same time providing the flexibility necessary to encourage creativity on the part of project designers.

3.3.4 Nonconforming Uses

Any use within the Specific Plan boundary which becomes nonconforming due to rezoning or zoning code amendment adopted by the City to be consistent with the requirements and standards of this Plan shall be subject to Chapter 22.38 of the Zoning Code, Nonconforming Uses, unless otherwise specified.

3.3.5 Design Review

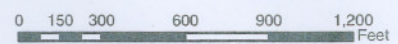
All new construction, new additions to existing buildings, and any other exterior improvements shall be subject to the design standards and guidelines set forth in this Specific Plan and require design review pursuant to the provisions of Sections 22.34.010 through 22.34.040 of the Zoning Code.



LAND USE

Downtown Martinez Specific Plan

- CIVIC - County Offices, Courts, other public uses
- DOWNTOWN CORE - Commercial, Retail, Office and Residential 29-43 Units/Acre
- DOWNTOWN NEIGHBORHOOD - Residential 12-35 Units/Acre
- DOWNTOWN SHORELINE - Residential 17-35 Units/Acre
- GRANDVIEW - Residential 7-17 Units/Acre
- Study Area Boundary
- Downtown Historic Overlay District
- North Downtown Shoreline
(Not adopted, reserved for future action)



Revised Figure 3-1

Chapter 4 – Downtown Core

This chapter defines the land uses, development standards and design standards and guidelines for the Downtown Core. This chapter is organized as follows:

- 4 Downtown Core
 - 4.1 Purpose
 - 4.2 Uses
 - 4.3 Zoning Districts
 - 4.5 Development Standards
 - 4.6 Design Standards and Guidelines

4.1 PURPOSE

The Downtown Core is the cultural and historic heart of Martinez. The quality of Downtown Martinez’s historic buildings and the relationship between these buildings creates a historic urban fabric unparalleled in Contra Costa County. The positive image and economic health of Downtown Martinez are strongly influenced by this historic character, and its protection is an essential part of assuring Martinez’s economic health and growth in the future. The intent of this Specific Plan is to create and encourage opportunities for a variety of commercial, residential, entertainment and cultural uses, including retail, office, residential and visitor-serving uses. The Downtown Core encourages a concentration of uses that generate activity during evenings and weekends as well as on weekdays. The retail uses in this area are intended to serve many of the daily shopping needs of Downtown residents and employees, as well as the specialty shopping needs of citywide residents, regional shoppers, and tourists.

The Downtown Core is divided into two sub-areas, consisting of 1) properties with frontage on Main or Ferry Streets and 2) all other properties. Standards for properties with frontage on Main Street and Ferry Street, the Downtown’s historic retail streets, are intended to reinforce this historic pedestrian retail pattern by concentrating active retail uses along these streets. Because of the concentration of buildings on Main Street and Ferry Street that contribute to Downtown’s historic character, these properties also fall into the Downtown Historic Overlay District (see Chapter 6 of this Plan), which provides standards for rehabilitation and new construction that are intended to preserve and enhance the historic character of the Downtown. Local landmarks such as the City Hall Apartments building, the Bergamini Building, the Old State Theater, and the McMahon Building help to define the district’s character. Preservation and rehabilitation of such structures, along with careful and compatible design of new development, is important in maintaining the area’s character and unique sense of identity.

Standards for the outer portion of the Downtown Core, consisting of properties without frontage on Main Street or Ferry Street, are intended to create a mixed-use pattern of

residential, office, cultural and small-scale light industrial and manufacturing uses that is in keeping with the traditional fine-grained mix of uses typical of a traditional Downtown.

4.2 USES

The uses in the Downtown Core are those allowed in the CC zone district, subject to the design guidelines of this Plan and the general requirements of the Zoning Code.

4.3 ZONING DISTRICTS

The existing CC district in the Downtown Core shall remain, and shall be expanded to include the SC and LI zoned area north of downtown. The existing PA zoned area from the north side of Green Street south to Masonic shall also remain, as a transition from the commercial area to the residential area.

4.5 DEVELOPMENT STANDARDS

All new development in the Downtown Core shall be accordance with the standards of the CC zone district for height, lot size, setbacks, etc. The CC District shall be amended to provide for a height limit of 40', or three stories, higher with a use permit, and a maximum density of 43 units per acre.

4.5.1 Maximizing Usage

In accordance with the “smart growth” principles stated in Section 1.3 of this Plan, the intent of the Downtown Core is to encourage two to three story buildings in the downtown, and to discourage one story buildings.

4.5.2 Parking

The majority of the Downtown Core falls within Martinez Parking District No. 1, in which nonresidential uses are not required to provide on-site parking. Residential uses must provide onsite parking in accordance with the standards of Chapter 22.34 of the Zoning Ordinance, and as allowed within the provisions of the Downtown Overlay district. Refer to Chapter 12 for additional off-street parking and loading requirements and standards.

4.5.3 Density

The basic density for residential shall be 29 units per acre, equivalent to R-1.5 zoning. The Planning Commission may approve up to a maximum density of 43 units per acre (equivalent to R-1.0 zoning), by use permit. In order to approve a density above the lower end of the density range, the Planning Commission would need to find that in

addition to meeting the above minimum requirements, the proposal is superior in terms of two or more of the following criteria:

- Design and appearance
- Minimizing impacts on adjacent public lands
- Providing onsite amenities for the future residents
- Preserving or creating view corridors.
- Utilizing green building practices to the maximum extent possible
- Providing a public amenity.

In order to approve a density at or near the upper end of the density range, the Planning Commission would need to find that the proposal is superior in terms of all or almost all of the above criteria.

4.6 DESIGN STANDARDS AND GUIDELINES

4.6.1 Character Defining Statement

The Downtown Core is the cultural, historic and retail center of Martinez and an attraction for the region beyond. Its significant signature buildings include the City Hall Apartments, Bank of Martinez, National Bank of Martinez, and the Hook, McMahan-Telfer, and Bergamini Buildings. Other culturally significant buildings in the District, such as Hilson’s Department Store and the Bell Telephone, Rankin, and McNamara-McMahon Buildings, have compromised their historic character through renovations. New construction should be in scale and architecturally harmonious with nearby historic buildings. The above listed signature buildings should be used for inspiration regarding design, form, detailing and site layout.

The design standards and guidelines for the Downtown Core are intended to enhance these signature buildings as well as their setting, since both buildings and context contribute to the character of a cohesive Downtown. In addition, the design standards and guidelines for the Downtown Core are intended to create a vibrant, pedestrian-friendly Downtown by encouraging pedestrian-oriented storefronts, human scaled spaces, and pedestrian amenities.

4.6.2 Site Design Standards

Building Orientation

Buildings should have strong street presence, with public entrances and activity areas oriented toward the street.

Setbacks

Commercial buildings should front directly onto public sidewalks with no intervening setback

Vehicular Access and Parking

On-site parking is discouraged, except for residential uses, and parking spaces should not have direct frontage on, nor vehicular access to or from Main Street or Ferry Street. Access points should be located on other streets.

Pedestrian Access

Primary access to buildings should be from the street or pedestrian walkways, not parking areas.

Site Furniture

Pedestrian amenities are encouraged, including benches, landscaped gathering areas, trash receptacles, etc. Design of improvements should be traditional and related to the signature buildings.

Chapter 5 – Civic

This chapter defines the land uses, development standards and design standards and guidelines for the Civic area. This chapter is organized as follows:

- 5 Civic
 - 5.1 Purpose
 - 5.2 Uses
 - 5.5 Development Standards
 - 5.6 Design Standards and Guidelines

5.1 PURPOSE

The intent of the Civic area is to provide a center for the existing functions and future expansion of the Contra Costa County government, including administrative, judicial and correctional facilities, and for federal, state and local civic facilities.

Because of the concentration of buildings on Court Street that contribute to Downtown’s historic character, a portion of the Civic area also falls into the Downtown Historic Overlay area, which provides standards for rehabilitation and new construction that are intended to preserve and enhance the historic character of the Downtown; see Chapter 7 of this Plan. Several landmarks that are either on or eligible for the National Register of Historic Places give considerable character to this area. These include the County Finance Building, County Courthouse, Post Office, Veterans’ Memorial Hall and Martinez Library. Preservation and rehabilitation of such structures, along with careful and compatible design of new development, is important in maintaining the area’s character and unique sense of identity.



Veterans’ Memorial Hall



Martinez Library

5.2 USES

The uses of the Civic area shall be those allowed in the Civic Zoning District, subject to the design guidelines of this Plan, and the general provisions of the Zoning Code.

5.5 DEVELOPMENT STANDARDS FOR THE CIVIC DISTRICT

5.5.1 General

All new development within the Civic area shall be in accordance with the standards of the Civic zone district for floor area, setbacks, heights, etc.

5.5.2 Parking

Refer to Chapter 12 for off-street parking requirements and standards. Portions of the area are dominated by large surface parking lots, and the use of shared parking structures or decks is encouraged if additional parking is to be provided.

5.6 DESIGN STANDARDS AND GUIDELINES

5.6.1 Character Defining Statement

The Civic area is the governmental and judicial center of Contra Costa County. Commercial and service uses are intended to be limited to those that are incidental to, and for the convenience of, the governmental and judicial uses. The significant, signature buildings that define this area include several historic structures: the County Finance Building, County Courthouse, Post Office, Veterans' Memorial Hall and Martinez Library. This area also includes contemporary buildings which may have historic references in detailing, massing, and fenestration. The area's historic buildings should be respected and complemented in any adjacent architecture. While taller buildings are anticipated in this area, respecting pedestrian scale is important. One example of how this can be accomplished is by stepping building heights from lower at the street to higher beyond. The signature buildings listed above should be used for inspiration regarding design, form, detailing and site layout.

5.6.2 Site Design Standards and Guidelines

Building Orientation

Building orientation shall provide for at least one significant street entrance elevation. Other frontages shall have a pedestrian scale and incorporate landscaping.

Vehicular Access and Parking

- 1) Parking shall typically be provided in parking garages with limited numbers of common access points.

- 2) On site parking (lots and structures) shall be located to the rear of the building.
- 3) Parking shall be accessed from public streets other than Court Street.

Pedestrian Access

- 1) Primary access to buildings shall be from the street or pedestrian walkways, not parking areas.
- 2) Pedestrian access through buildings in this district is encouraged where security considerations allow.

Site Furniture

Pedestrian amenities are encouraged, including benches, landscaped gathering areas, trash receptacles, etc.

5.6.3 Architecture

Style

- 1) Existing buildings of historic merit should be restored or maintained in a style that reflects the original appearance of the building.
- 2) New buildings in this district should exhibit a modern “formality” befitting government and judicial uses. Government buildings should exhibit a formal character through contemporary architectural forms. The architectural style of new buildings should have a contemporary appearance but utilize elements that complement the historic character of Martinez, using the signature buildings as a source of design inspiration. Refer to Chapter 6, Downtown Historic Overlay District, for more information on historic architectural styles.

Scale

The portion of buildings and improvements with an orientation to pedestrian spaces should be at a pedestrian scale. Other parts of buildings, which do not include pedestrian spaces, may be larger or taller in scale.

Entry Location and Treatment

Building massing should highlight the location of building entries. Primary pedestrian entries should be clearly expressed and should be recessed or framed by a sheltering element such as an awning, arcade, porch or portico. Greater height may be used to accentuate entries in the form of tower elements, tall openings, or a central mass at an entry plaza.

Roof Design

Roof design should reflect and complement significant buildings in the area.

Detailing

Detailing of existing buildings should be accurate to the original style of the building. New buildings should use detailing reflective of the signature buildings in the area .

Colors and Materials

Muted earth tones and traditional materials such as stone, masonry or stucco should prevail, with brighter colors limited to trim areas.

5.6.4 Landscaping

Plant Types

Plant types may be drawn from a broad plant palette, but should be suitable for the climate and exposure in which they are to be placed. Plant selection should be in accordance with Section 22.35.040 of the Zoning Code.

Scale

Pedestrian scale plantings should prevail, with larger plantings used as accents.

Hardscape

Paved areas may draw from a broad range of materials, designs and finishes that are complementary to the building architecture.

Chapter 6 – Downtown Historic Overlay District

This chapter defines the development standards and design guidelines for the Downtown Historic Overlay District. This chapter is organized as follows:

- 6 Downtown Historic Overlay District
 - 6.1 Purpose, Applicability and Context
 - 6.2 Administration
 - 6.3 Design Standards
 - 6.4 Historic Architectural Styles
 - 6.5 Design Guidelines for Rehabilitation of Commercial Buildings
 - 6.6 Design Guidelines for Infill Construction in the Downtown Historic Overlay

6.1 PURPOSE, APPLICABILITY AND CONTEXT

6.1.1 Purpose

The general purpose of the Downtown Historic Overlay District is to promote the preservation and rehabilitation of historic commercial, civic and mixed-use buildings in the Downtown Core and Civic Districts, and to provide for new infill construction consistent with the historic character of the District. In addition, the Overlay District has three specific purposes:

- a) to establish mandatory provisions for the preservation of buildings individually listed on the National and/or State Register of Historic Places (referred to below as Listed Buildings), or buildings which become so listed in the future;
- b) to establish advisory Design Review guidelines for the rehabilitation of structures and new infill construction in the historic district; and
- c) to provide a voluntary option to owners of qualified properties -- the more flexible provisions of the State Historical Building Code. The presence of a local historic district also creates the possibility for the City to establish a Mills Act program, which could provide property tax relief for owners who restore and maintain historic properties.

6.1.2 Applicability

The provisions of this Chapter shall apply to all properties located within the Downtown Historic Overlay District.

6.1.3 Context

Historic districts, as opposed to the designation of individual structures as landmarks, are appropriate when the concentration of historic structures in a defined area results in a cumulative level of historic character greater than the sum of the individual structures. Figure 6-1 shows the Downtown Historic Overlay District in the context of previously identified historic sites or buildings. The Historic Overlay District includes several historic civic buildings that are individually listed on the National Register of Historic Places, such as the County Finance Building and the Contra Costa Courthouse.

In addition to these civic landmarks, the District has a high proportion of commercial buildings that still exhibit most of their historic character, based on field observation by the consultant team as part of the Specific Plan process; Figure 6-1 identifies such structures within the Overlay District as “potential contributing structures.” While they include many architectural gems, what is most significant about these structures is the way they collectively create a streetscape that is remarkably unchanged from the boom years of Downtown Martinez’s industrial expansion in the 1910s and 1920s.

However, several historic commercial buildings in Downtown Martinez are marred by façade alterations from the 1950s and 60s that completely obscure their historic character. The result is an area with pockets where the historic character of the area has been lost. Restoring this historic character will be challenging. Many of the structures are of unreinforced masonry construction, which are generally expensive to rehabilitate to modern seismic standards while still maintaining historic qualities. Generally, it is difficult for building owners to recoup the costs of these improvements through increased rents. Consideration therefore should be given to financial incentives, such as the establishment of a Mills Act program, which could provide property tax relief for owners who restore and maintain historic properties. Flexibility will also be important in the design review process to maximize the retention of historic elements while at the same time including a realistic consideration of financial feasibility. Failure to do so could result in further modifications that are insensitive to historic values, on the one hand, or continuation of the status quo and additional deferred maintenance in the event that cost-effective improvements cannot proceed.

There are also a number of structures in the downtown area that exhibit significant deferred maintenance. There are also numerous underutilized or vacant properties that are potential sites for new construction. As has been demonstrated in other areas of Northern California with a substantial stock of Victorian and Edwardian structures, such as San Francisco, modern construction techniques are compatible with designs that mimic and retain much of the look and feel of these historic eras. Design of new structures in the downtown should take advantage of these architectural approaches, and avoid more modern design approaches that would dilute the remaining historic qualities of the downtown area.

Many Downtown structures were identified as cultural resources in the City’s 1983 Historic Resource Inventory (included in this Plan as Appendix E), which was field-reviewed within the Historic Overlay District as part of this Specific Plan. In contrast to the cultural resource inventory, however, which includes the sites of historic buildings that have been demolished or altered beyond recognition, contributing structures to a historic district should have visible and intact historic character.

The resource list at the end of this Chapter identifies additional information regarding historic preservation and procedures for establishing local, state and national historic districts.

6.1.4 Additional Measures

The intent of the Historic Overlay District is to preserve historic buildings to the maximum extent feasible. Therefore, no building more than 50 years old may be demolished before first investigating all feasible methods of re-using the structure. The City should consider preparing an ordinance providing that demolition of older buildings in the historic district should be allowed only after full evaluation of the feasibility of all alternatives available to the property owner that would allow the retention of the original structure: federal and/or state tax incentives, alternative methods of construction/rehabilitation etc. This ordinance should be developed in concert with the proposed ordinance governing unreinforced masonry buildings, but should also apply to non-URM's.

The City should also update regularly the 1982 Historic Resource Inventory, in cooperation with the Historic Society, and incorporate the additional listings in Table IV.G-2 (Cultural Resources) from the EIR, where appropriate.

6.2 ADMINISTRATION

This historic overlay district shall be implemented by adopting a Historic Overlay District as part of the City's Zoning Code, containing the following two paragraphs:

“All proposals for building demolition, or substantial modification of Listed Buildings shall be subject to the mandatory criteria below – Section 6.3.1 Secretary of the Interior's Standards for Rehabilitation -- in addition to the standards of the individual Specific Plan District and the City's standard Design Review process, as detailed in Chapter 22.34 of the Zoning Code; and the advisory guidelines of this overlay district.

All proposals for substantial modification or demolition of other structures, or new construction, in the Downtown Historic Overlay District shall be subject to the standards of the individual Specific Plan District and the City's standard Design Review process, as detailed in Chapter 22.34 of the Zoning Code, and the advisory guidelines of this overlay district. All such proposals shall be referred to the Martinez Historic Society for comments as part of the Design Review process. Where appropriate, the City may use other resources, such as architectural historians, for supplemental advisory input to the Design Review process.”

Further, the new Historic Overlay zone district shall be applied by rezoning the area within the boundaries of the Historic Overlay District, as shown in this Plan, to the new H District.

6.3 DESIGN STANDARDS AND GUIDELINES

The City of Martinez treasures its heritage of historic structures. The historic architecture of the Downtown is one of the City's most important resources. Establishing and enforcing guidelines for the treatment of historic buildings and structures in historic districts will help maintain this resource. The presence of these guidelines serves a dual purpose: to protect the heritage of the City of Martinez and to protect the interests of property owners and residents.

While the design standards for Listed Buildings and guidelines in this chapter are a part of the Downtown Martinez Specific Plan, they are designed to be a useful tool to any property owners seeking advice about the appropriate treatment of their historic property. This Chapter primarily addresses commercial, mixed-use, and civic structures. Historic residential structures are discussed in Chapter 8, Downtown Neighborhood. The publications listed at the end of this Chapter and in Chapter 8 contain a wealth of information on the treatment of historic building materials not covered in these guidelines.

6.3.1 Secretary of the Interior's Standards for Rehabilitation

The Secretary of the Interior's Standards for Rehabilitation are ten basic principles created to help preserve the distinctive character of a historic building and its site, while allowing for reasonable change to meet new needs. These standards are mandatory for Listed Buildings. These Standards are the basic principles from which the advisory Design Guidelines in this Chapter were developed.

The Standards are:

- a) A property shall be used for its historic purpose or be placed in a new use that requires minimal change to the defining characteristics of the building and its site and environment.
- b) The historic character of a property shall be retained and preserved. The removal of historic materials or alteration of features and spaces that characterize a property shall be avoided.
- c) Each property shall be recognized as a physical record of its time, place, and use. Changes that create a false sense of historical development, such as adding conjectural features or architectural elements from other buildings, shall not be undertaken.
- d) Most properties change over time; those changes that have acquired historic significance in their own right shall be retained and preserved.
- e) Distinctive features, finishes, and construction techniques or examples of craftsmanship that characterize a property shall be preserved.
- f) Deteriorated historic features shall be repaired rather than replaced. Where the severity of deterioration requires replacement of a distinctive feature, the new feature shall match the old in design, color, texture, and other visual qualities and, where possible, materials. Replacement of missing features shall be substantiated by documentary, physical, or pictorial evidence.

- g) Chemical or physical treatments, such as sandblasting, that cause damage to historic materials shall not be used. The surface cleaning of structures, if appropriate, shall be undertaken using the gentlest means possible.
- h) Significant archeological resources affected by a project shall be protected and preserved. If such resources must be disturbed, mitigation measures shall be undertaken.
- i) New additions, exterior alterations, or related new construction shall not destroy historic materials that characterize the property. The new work shall be differentiated from the old and shall be compatible with the massing, size, scale, and architectural features to protect the historic integrity of the property and its environment.
- j) New additions and adjacent or related new construction shall be undertaken in such a manner that if removed in the future, the essential form and integrity of the historic property and its environment would be unimpaired.

6.3.2 State Historical Building Code

The owner of a Listed Building, or other qualified structure, may voluntarily choose to utilize the provision of the State Historical Building Code.

The purpose of the State Historical Building Code (SHBC) is to provide regulations and standards for the rehabilitation, preservation, restoration (including related reconstruction) or relocation of historical buildings, structures and properties deemed of importance to the history, architecture, or culture of an area by a governmental jurisdiction. Such standards and regulations are intended to facilitate the restoration or change of occupancy to preserve their original or restored elements and features, to encourage energy conservation and a cost effective approach to preservation, to provide for reasonable safety from fire, seismic forces or other hazards, and to provide reasonable availability and usability by the physically disabled.

The State Historical Building Code is set forth in Sections 18950 to 18961 of the California Health and Safety Code (H&SC). The 2001 California Historical Building Code is the most recent published regulation of the State Historical Building Code. The California Historical Building Code is Part 8 of Title 24 of California's Code of Regulations. The California Historical Building Code is also published as Chapter 34, Division II of the California Building Code.

Some of the issues the SHBC addresses, all of which facilitate sensitive and cost-effective rehabilitation, include the following:

- a) Accessibility - Both the Americans with Disabilities Act of 1992 (ADA) and the SHBC make provisions for reasonable levels of equivalency for, and - under special circumstances - exemption from, accessibility mandates.
- b) Seismic/Structural - SHBC governs these issues, permitting design based on real values of archaic materials, and solutions based on engineering principles and judgment rather than on prescriptive formulas.

- c) Energy - Qualified historic buildings are exempt from California energy standards, which most vintage structures cannot meet without alteration or loss of historic features.
- d) Triggers - The "triggers" for full upgrading to current standards, with respect to length of vacancy, change of occupancy, or percentage of value of the work proposed, which otherwise exist in the building code, are modified by the SHBC, which concentrates instead on the sensitive resolution of genuine safety considerations.

The SHBC gives local building officials flexibility in interpreting “reasonably equivalent” alternatives to code requirements based on the unique circumstances of each building, site or property. The code is for the most part a set of performance criteria based on reference to regular code. To facilitate standardization of application of the code, the State Historical Building Safety Board is given authority for appeals and review functions.

6.3.3 Design Review Considerations Applicable to Unlisted Properties

For unlisted structures, including those identified as potential contributing properties in Figure 6-1, as well as new construction, historic considerations will be integrated into the Design Review process on a case-by-case basis taking into account the nature of the structure, its context, and cost-effective opportunities to enhance historical values. Generally, this will include consideration of the architectural style of the structure and its neighbors, with a goal of generally compatible design. A summary of existing historic civic and commercial architectural styles in Downtown Martinez is set forth in the next section.

6.4 HISTORIC CIVIC AND COMMERCIAL ARCHITECTURAL STYLES IN DOWNTOWN MARTINEZ

6.4.1 Neoclassical (1900-1920)

The final stage of classical European architecture, in the late 18th and 19th centuries, emphasized strict use of the Classical orders (Doric, Ionic, Corinthian, etc.), monumentality (e.g., two-story columns supporting a portico) and relatively little ornamentation. The term Neoclassical applies both to this late classical period and to a revival style from the early 20th century, based mostly on Greek and a few Roman precedents. This Neoclassical revival style produced symmetrical façades of monumental proportions. Colossal pedimented porticos were flanked by a series of pilasters. The arch was not used, and enriched moldings are rare. Windows are undivided, and the statues on the roof line typical of earlier Classical phases are no longer seen. The County Finance Building (above right), a National Register historic structure built as a



courthouse in 1901, exemplifies the Neoclassical style. This landmark building awaits funding for restoration; its original dome was removed in the 1950s.

6.4.2 Mission Revival (1900-1940)

The Mission style originated in southern California and was the first style to spread eastward from the West. The style was considered the "California counterpart" to the Georgian-inspired Colonial Revival popular in Northeast. Most famously, the style was adopted by both the Southern Pacific and Santa Fe Railways for train stations, resort hotels, and other rail corridor buildings, essentially as an effort to "theme" the Southwest for eastern travelers. The style includes Mission-shaped dormers and/or roof parapets; wide, overhanging eaves, exposed rafters, red-tiled roofs, stucco walls, and arched windows and doors on the ground level.



6.4.3 Two-Part Commercial Block (Circa 1865-1920)

The two-part commercial block was basically built with a public use on the first floor, such as a retail store, bank, insurance office, or lobby, and more private uses on the floors above, such as offices, meeting rooms, or hotel rooms. Between 1865 and 1880, most examples were constructed of brick supported by cast iron or heavy timbers and had little ornamentation other than corbelling or protective cornices. During the Victorian era (1880-1895), these structures were often built of cast iron frame with brick walls and were embellished with elaborate ornamentation. Typical features included Italianate brackets; large scale surrounds or hoods on upper story windows, often with keystones or pediments, elaborate cornices and string courses; quoining; and/or turrets, towers or parapets interrupting the roof line. Lower story windows were generally large showcase shapes, while upper story windows often appeared in unusual arched shapes or oriel configuration, or may have been accentuated by pronounced bays.



6.4.4 Tripartite Renaissance Revival/Commercial Vernacular (Circa 1895-1920)

Most “Main Street” commercial buildings from the first quarter of the twentieth century can be categorized as “commercial vernacular.” Typically of brick construction with finished red, buff, or brown brick used in the façade, these buildings were one to three stories and could be freestanding or attached to buildings on either side. They consist of three parts: a ground level of storefronts, a middle level of windows, and a parapet or cornice. On a few structures, detailing was derived from the Renaissance Revival and was executed either in differently colored brick or in brick patterns, terra cotta, or cast stone. Storefronts were usually composed of a bulkhead (wood in the earliest examples, tile or Bakelite in later eras), display windows, a glazed entry, and a transom that spanned the entire composition. Upper story windows could be flat headed or have segmented arches.



6.4.5 Art Deco (circa 1924-1940)

Art Deco architecture of the late 1920s and 1930s strove to express a sense of speed and modernity while still integrating traditional forms and decorative elements. Its initial phase featured a pronounced verticality, expressed by stepped piers and cornices, repeated triangular and chevron motifs, and the frequent use of tall marquees to catch the eye of speeding motorists. Even after the stock market crash of 1929 put an end to the skyscraper boom, designers of more modest, low-rise buildings such as gas stations used Art Deco motifs to express a horizontal thrust, and curves reminiscent of airplanes, locomotives and ocean liners. Raised bands of horizontal moldings were often paired or tripled. Other features included canopies, pipe railings, rounded counters, porthole windows, and openings glazed with glass brick. Metal elements were popular, such as metal casement windows, decorative sheet metal panels, and stainless steel storefront trim.

Public buildings during this era, often constructed as part of the Works Progress Administration program, superimposed Art Deco piers and decorative elements on traditional Classical and Renaissance Revival building forms in a style that came to be known as WPA Moderne (circa 1931-40). The Contra Costa County Courthouse (right), a National Register historic structure completed in 1931, is an outstanding example of this style.



6.5 DESIGN GUIDELINES FOR REHABILITATION OF HISTORIC COMMERCIAL BUILDINGS IN THE DOWNTOWN HISTORIC OVERLAY DISTRICT

6.5.4 Site Design

Historically, commercial areas in the Downtown Specific Plan area had a consistent building line tightly aligned against the sidewalk. Buildings were typically attached to one another. Off-street parking lots were not part of the original urban fabric but were later added on vacant lots, generally to the rear or side of the use. The sidewalk and street trees in front of a building form an integral part of its setting. Preservation of these uninterrupted blockfronts is essential to maintaining the historic, pedestrian-friendly character of Martinez’ historic commercial area.

Guidelines

Landscaping and Site Design

Mature trees, particularly street trees in the public right of way, should be preserved whenever possible.

Parking Areas and Access

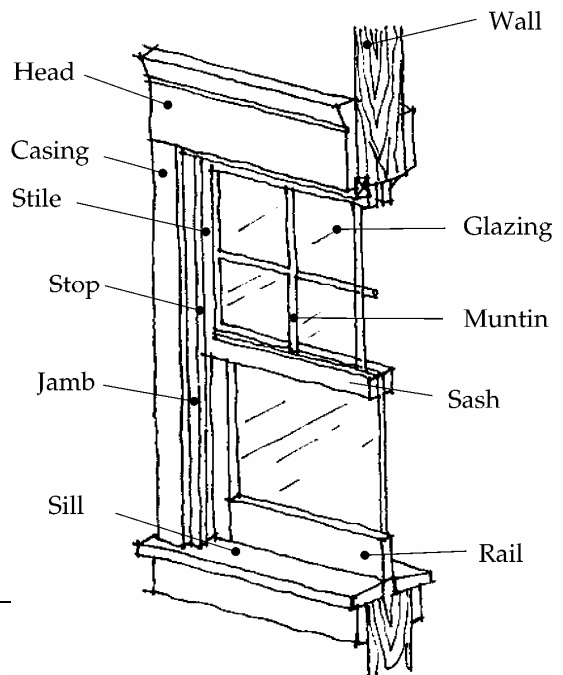
Parking areas and driveways should be located to the rear of commercial structures.

- a) New parking areas should be screened from public view by appropriate fencing or planting strips.
- b) Entrances for commercial parking areas should be located from streets other than those faced by the primary architectural façade of the building wherever possible. Vehicular entrances to parking areas from Pedestrian-Priority Streets are prohibited. (See Chapter 13, Circulation).

6.5.5 Openings

The pattern of windows, doors and other openings on the façades of a historic structure strongly define its character. Changing these elements in an inappropriate manner has a strong negative impact on the historic character of the structure. Windows and doors define character through their shape, size, construction, arrangement on the façade, materials and profile.

PARTS OF A WINDOW



Maintaining historic windows and doors often makes good economic sense, as they typically had a much longer life span than modern replacement windows. For instance, adding interior storm windows and doors can increase energy efficiency while preserving the historic character of a structure and saving money.

Guidelines

- a) The arrangement, size and proportions of historic openings on a façade should be maintained.
 - 1) Filling in or altering the size of historic openings, especially on primary façades, is discouraged.
 - 2) Adding openings to historic façades is discouraged.
- b) The materials and design of historic windows and doors and their surrounds should be preserved.
 - 1) Repair windows or doors wherever possible instead of replacing them.
 - 2) When replacement of these windows is necessary, replacement windows should match the historic windows in size, shape, arrangement of panes, materials, methods of construction, and profile. Local window and door shops can custom-fabricate replacement wood windows that are more faithful to historic windows' appearance than new manufactured windows.
- c) If energy conservation is the goal, interior (preferred) or exterior storm windows or doors, rather than replacement windows or doors, should be used.
- d) Awnings and shutters should be similar in materials, design and operation to those used historically. Awnings and shutters should only be utilized on openings in structures where their use was likely in historic periods.

6.5.6 Roofs

The character of the roof is a major feature for most historic structures. Similar roof forms repeated on a street help create a sense of visual continuity for the neighborhood. Roof pitch, materials, size, orientation, eave depth and configuration, and roof decoration are all distinct features that contribute to the character of a roof.

Characteristics of Historic Roofs

Traditionally, Martinez's historic commercial structures commonly had flat roofs. These roofs were necessary to the form of the historic commercial building and should be maintained. Roofing materials for flat-roofed buildings, on which the roofing material is not visible, are generally not a character-defining feature of a structure. Of greater concern is the placement and screening of roof-mounted mechanical equipment, since this equipment is jarringly out of character with historic architecture. Commercial structures built in the Renaissance Revival style often had terra-cotta tile roof elements or parapet details.

Guidelines

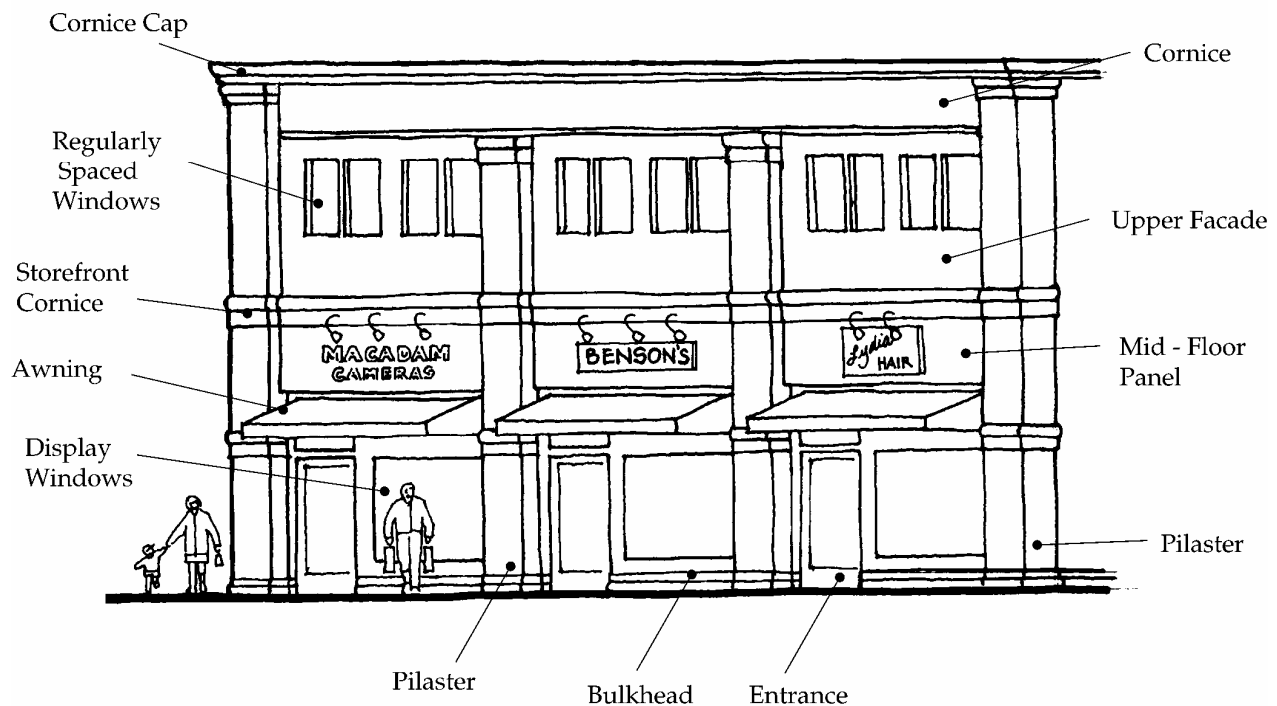
- a) The historic roof form should be preserved.
- b) The historic eave depth and configuration should be preserved.
- c) Historic specialty roofing materials, such as tile or slate, should be preserved in place or replaced in kind.
- d) Roof and eave details, such as rafter tails, vents, parapets, dormers, and other architectural features, should be preserved.
- e) Roof mounted equipment should be screened from view of pedestrians and users of adjacent buildings by either a building parapet or mechanical penthouse.

6.5.7 Architectural Details and Building Materials

The characteristics of the primary building materials, including the scale of units in which the materials are used and the texture and finish of the material, contribute to the historic character of a building. For example, the color and finish of historic stucco is an important finish of Renaissance Revival structures.

Architectural details add visual interest, distinguish certain building styles and types and often showcase superior craftsmanship and architectural design. Features such as lintels, brackets and columns were constructed with materials and finishes that are associated with particular styles and are character-defining features as well.

ELEMENTS OF A RETAIL STOREFRONT FACADE



Guidelines

- a) Original building materials and architectural features should be preserved.
- b) Deteriorated materials or features should be repaired in place, if possible.
- c) When it is necessary to replace materials or features due to deterioration, replacement shall be in kind, matching materials and design.
- d) Materials, such as masonry, which were not originally painted, should remain unpainted.
- e) Original building materials and details should not be covered with stucco, metal panels or screens, or other materials. Where they have been applied, such materials should be removed and the building restored to its historic appearance.

6.5.5 Additions

Poorly planned additions can destroy a structure's historic character. Additions can not only radically change the appearance of a structure but can also result in the destruction of much significant historic material in the original structure. Careful planning of additions will allow for the adaptation of historic structures to meet the needs of the current owner, while preserving their historic character and materials.

Guidelines

- a) Additions should be located in the rear of the structure wherever possible, away from the main architectural façade.
- b) Additions should be compatible in size and scale to the original structure, although subordinate in massing.
- c) Additions should use similar finish materials and fenestration patterns as the original structure.
- d) Rooftop additions should be located to the rear of the structure if possible.
- e) Additions should be differentiated from the original structure through their details or massing, communicating clearly that the addition is in fact new construction.

6.6 DESIGN GUIDELINES FOR INFILL CONSTRUCTION IN THE DOWNTOWN HISTORIC OVERLAY DISTRICT

6.6.4 Site Design and Building Location

Historically, commercial buildings on Main and Ferry Streets were built directly abutting the public sidewalk, without front setbacks. Building widths varied within a range governed by lot sizes and available building technology. New construction should reflect these historic patterns.

Guidelines

- a) The façades of new structures in commercial areas should maintain the building line of existing historic structures along the street front.
- b) New structures should reflect the traditional widths of historic structures in the area.
- c) New structures which are wider than the traditional width should be designed to read as smaller modules reflecting the traditional building widths.
- d) Onsite parking is discouraged and should not be accessed from the primary street frontage.

6.6.5 Building Mass, Scale and Form

Historic commercial areas in the Downtown Specific Plan area were generally composed of one- to three-story flat roof structures composed as rectangular solids.

Guidelines

- a) New structures should be similar in scale to historic structures within the area.
- b) The basic building form for new commercial or mixed-use structures should be a simple rectangular solid.
- c) A flat roof is the preferred roof form.

6.6.6 Materials and Details

Materials commonly used on façades of historic commercial structures in Martinez included brick, stucco and masonry. Architectural details were usually embellishments added to the solid plane of the façade or parapet details rising from it. Echoing these traditions in the design of new construction will help preserve the distinctive character of Downtown Martinez’s historic commercial area.

Guidelines

- a) Building materials should be similar, or at least appear similar, to those used historically.
- b) Generally, architectural details should be arranged to emphasize the vertical aspect of façades.
- c) Architectural details should echo, but should not exactly mimic, details found on historic façades.
- d) The colors of permanent finish materials should be similar to those used historically.
- e) The use of architectural detail to break up the visual mass of outsized buildings is encouraged.

6.6.7 Openings, Storefronts and Entries.

The character of historic commercial block faces is largely defined by the storefronts, entryways, windows and doors, which create street-level interest for pedestrians and passersby. While a historic commercial block face might be composed of a Mission Revival structure, an Art Deco structure, and several Italianate structures, all of these structures would have presented a similar face to the sidewalk, with large expanses of glass storefront windows, welcoming well-marked entryways, and largely regular, horizontally massed windows. Most historic commercial structures employ these basic architectural elements to create a welcoming retail experience for passerby while expressing their architectural style through details and materials. It is essential to the character of historic commercial districts that new structures utilize these common elements to ensure that the character of the area is not lost.

Guidelines

- a) On the ground floor of new commercial or mixed-use structures, a majority of the primary architectural façade should echo traditional retail storefronts.
- b) The ground floor of the primary architectural façade should be composed primarily of transparent elements.
- c) Recessed entryways are strongly encouraged for primary entrances on the ground floor level.
- d) Primary entryways should be clearly marked through the use of important defining architectural elements, such as transoms, awnings, lintels or surrounds.

- e) New ground-level façades should echo the widths of existing historic storefront bays in the area through their use of architectural detail and articulation.
- f) Upper-story windows should be regularly spaced and horizontally massed on the primary architectural façade.
- g) On structures occupying corner lots, corner entryways with strong architectural emphasis are encouraged.

ADDITIONAL RESOURCES FOR HISTORIC PRESERVATION

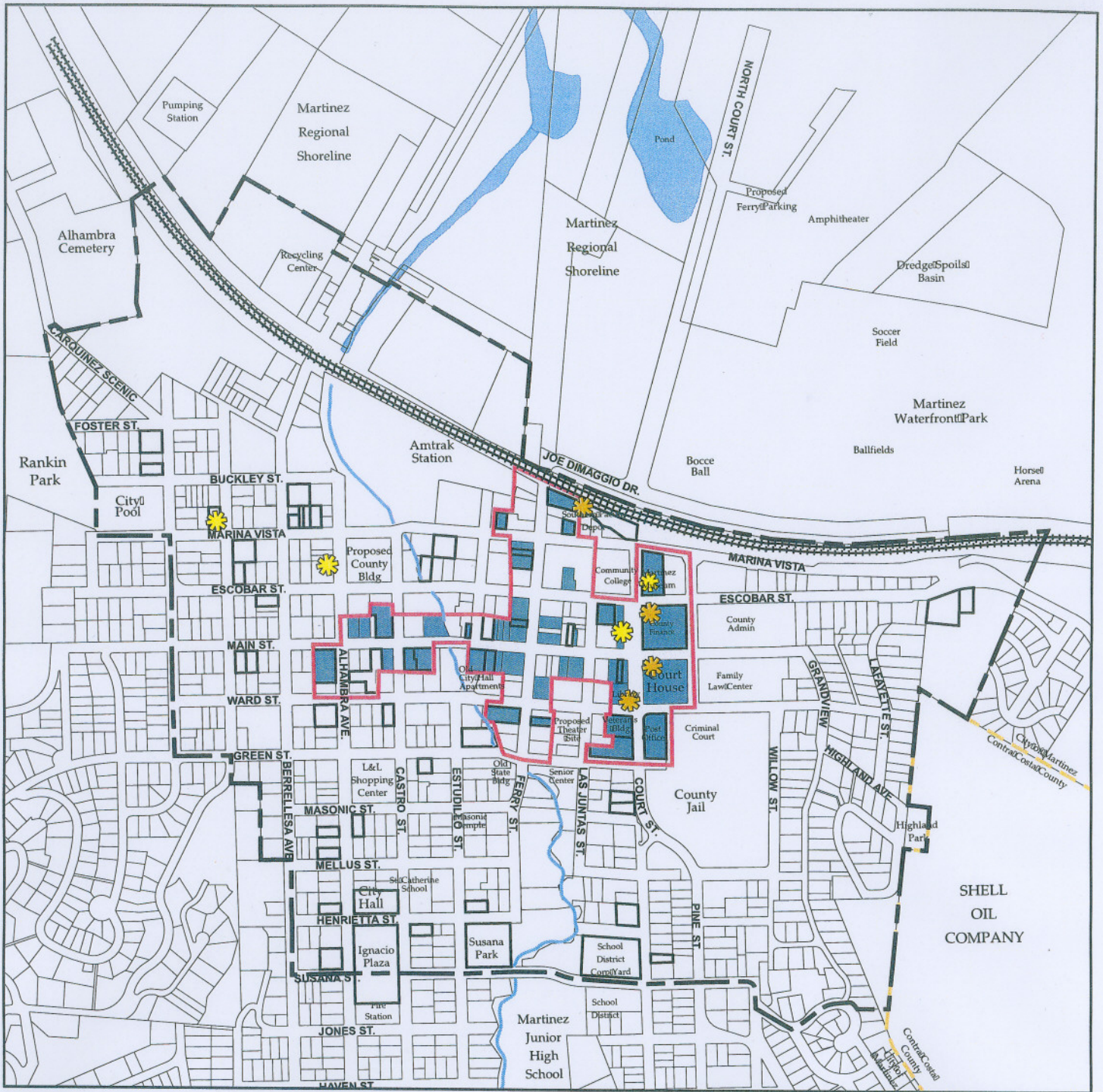
Cassity, Pratt. *Maintaining Community Character: How to Establish a Local Historic District* (Washington: National Trust for Historic Preservation, n.d.)

Living on Main Street: Lessons in Livability from Oregon's Downtown. (Portland, Liveable Oregon, Incorporated, 1994).

State of California, Office of Historic Preservation. Historic Preservation Workshop Materials. http://ohp.parks.ca.gov/default.asp?page_id=23125

State of California, Office of Historic Preservation. Historic Preservation Training For Local Government, Commissions, Boards And Staff. <http://ohp.parks.ca.gov/pages/1054/files/hp%20training%20-%20vallejo%20handouts.pdf>

Tyler, Norman. *Historic Preservation: An Introduction to Its History, Principles and Practice* (New York, W. W. Norton, 2000).



LEGEND

- Study Area Boundary
- Rail Road
- Property Lines
- Alhambra Creek
- Historic Resources – 1982 Inventory
(Source: 1982 Inventory of Historic Resources)
- Potential Contributing Properties in Proposed Historic District
- Proposed Downtown Historic Overlay District
- Buildings that are eligible, have been determined eligible, or appearing eligible for listing in the National Register of Historic Places
- Sites or structures that may become eligible for listing in the National Register of Historic Places
(Source: California Office of Historic Preservation's Directory of Properties in the Historic Property Data File and the Revised Historic Resources Inventory, Contra Costa County, California-Contra Costa County Planning Department, 1989)

Fig.6-1 HISTORIC RESOURCES

Downtown Martinez Specific Plan

Martinez, California



Calthorpe Associates
 URBAN DESIGNERS
 PLANNERS
 ARCHITECTS
 Berkeley, California
 City of Martinez, California

Chapter 7 – Downtown Neighborhood

This chapter defines the land uses, development standards and design standards and guidelines for the Downtown Neighborhood. This chapter is organized as follows:

- 7 Downtown Neighborhood
 - 7.1 Purpose
 - 7.2 Uses
 - 7.3 Zoning Districts
 - 7.4 Density
 - 7.5 Development Standards
 - 7.6 Historic Residential Architectural Styles
 - 7.8 Design Guidelines

7.1 PURPOSE

The intent of the Downtown Neighborhood is to provide for a variety of residential uses in an environment that is typically residential in character. This area serves as a transition to the predominantly single-family residential neighborhoods to the west and south.

The City’s Historic Resource Inventory identified some of the homes and sites in this area as having historic significance. Any development within this area should give careful attention to the preservation and rehabilitation of these historic resources. Please see Sections 7.6 and 7.7.3 for additional discussion of historic preservation issues and resources.

7.2 USES

The uses in the Downtown Neighborhood, are those allowed in the zone districts of PA, R-3.5, R-2.5 and R-1.5, subject to the design guidelines of this Plan and of the Zoning Code.

7.3 ZONING DISTRICTS

The existing zoning districts of PA, R-3.5, R-2.5, R-1.5 and DO shall remain on the Zoning Map within the Downtown Neighborhood area. Owners of individual parcels may apply for a rezoning, so long as the proposed density is within the density range specified in this chapter. Such rezoning requests will be considered by the Planning Commission on a case-by-case basis.

7.4.1 DENSITY

The allowable density range for the Downtown Neighborhood shall be 12 to 35 units per acre (equivalent to R-3.5 to R-1.5-DO zoning).

7.5 DEVELOPMENT STANDARDS FOR THE DOWNTOWN NEIGHBORHOOD AREA

All new development in the Downtown Neighborhood area shall be in accordance with the standards of the applicable zoning.

7.6 HISTORIC RESIDENTIAL ARCHITECTURAL STYLES

The following architectural styles are prevalent in the historic neighborhoods of Downtown Martinez:

7.6.1 Italianate (1850s-1880s)

The Italianate style is a wood-frame adaptation of stone structures built in 17th century Italy. Italianate houses are characterized by a low-pitched or flat roof with wide, overhanging and bracketed heavy supporting brackets under wide eaves. These two- to three-story wood-framed structures have a typically rectangular massing, often picturesquely arranged in imitation of Italian villas that were enlarged over generations. Generally, Italianate style structures have corniced eaves, angled bay windows and Corinthian-columned porches. Decoration is elaborate and may include paired brackets and cornices and even a square cupola. Traditional stone details were recreated in redwood or Douglas fir, using the manufactured trim that was becoming available in the period. Openings are tall and narrow, typically with double-hung windows with hood moldings and heavily molded doors.



In the Bay Area, a smaller version of the Italianate house was developed. A one-story building with front bay, bracketed cornice, pedimented window, simplified portico and raised first floor, the details are the same as the two-story type, but the overall proportions are different.



7.6.2 Stick/Eastlake (1860s-1890s)

Stick-Eastlake structures were plain, simple and relatively modern. Because they lacked the ornamentation of other styles like the Queen Anne house, they did not last long during an era which delighted in fanciful adornments. Stick houses were characterized by a large, ornamental truss under the gable eaves of a house, and often included square bays, flat roof lines and free-style decorations. The Stick Style emphasizes straight lines and right angles. Flat, narrow boards are nailed to the outside of the building to repeat and reinforce the structural frame within. Diagonal braces parallel to the façade frame the front porch. The roof projects over the front of the house so that the gable end forms a separate plane. Rectangular bay windows, which are easier to construct, replace the slanted bay of the Italianate. The horizontal siding is reoriented to vertical and diagonal positions, especially around the bay window where the structure is exaggerated by the board and batten technique. Abundant redwood and Douglas fir supplies were carved with scrollsaw and jigsaw into intricate flat shapes.



A decorative variation on the Stick style, known as “Eastlake”, carried the exploration of the properties of wood to the three-dimensional. Knobs and newels turned out by chisel, lathe and gouge, and most closely resembling table legs, typify this architectural mode mistakenly named for British designer Charles Eastlake. The plan itself is usually reminiscent of its Italianate precursor.

7.6.3 Queen Anne Revival (1880s-1890s)

Queen Anne buildings are characterized by complex roofs of fairly steep pitch; combinations of siding materials such as clapboard and patterned shingles; rounded and three-sided slant bays of one or more stories; towers and turrets; porches and balconies, sometimes rounded in configuration, and by the incorporation of ornamental elements such as turned wood columns and spindles, sawn bargeboards and brackets, stained and leaded glass, and molded plasterwork. Examples range from small, L-shaped cottages with a bay window on the projecting wing and a porch with a couple of columns and brackets on the perpendicular wing, to two-and-a-half-story “tower houses” with a profusion of architectural elements and ornamental embellishments. Carpenter Gothic was a variant that became quite popular with



wood-framed churches. Eastlake or Stick-influenced houses of this era are generally similar in massing, with squared bays and a linear, two-dimensional quality to their ornament.

In the 1880s, a rowhouse version of the Queen Anne style emerged up in cities across the U.S and in the Bay Area., adapting the popularity of the picturesque to a broader range of budgets. The demands of a growing population for new housing were accommodated by a proliferation of Queen Anne cottages. The Queen Anne cottage is a one-story building dominated by an oversized gable, which may be shingled, framed with intricate bargeboard, pierced by flashed glass windows, stamped with a sunburst and topped with a proud finial. The gable overwhelms the front bay window, creating cur-away corners and a recessed porch. Special ornamentation evolved for each of these new features.

7.6.4 American Foursquare/Classic Box (1890-1910)

American Foursquare houses are recognized by their square proportions, often given a horizontal emphasis by roof or siding treatments; by the nearly always present hipped roof and dormer; and by a front porch either recessed or attached, spanning all or part of the façade. Columns suggestive of the classical orders, dentils and other traditional moldings, cornerboards treated as pilasters, and boxed cornices tied these homes to the tradition of the American Colonial Revival; they can also be referred to as a “Classic Box.” A front porch, often recessed into the façade, was a ubiquitous element.



Typical interior details include hefty ceiling beams, waist-high wainscoting, and classic columns in the archways – all in a dark finish. Hardwood floors with inlaid parquet border emphasized the regularity of the floor plan.

A smaller Bay Area style which grew out of the Colonial Revival genre is the Neoclassic Rowhouse. A one-story house on a raised foundation, with a hipped roof and dormer window, the Rowhouse shares many classic details with the Classic Box. Its form, however, is more reminiscent of its forebear, the Queen Anne Cottage. Although “rowhouse” suggests attached units, it is used here to mean detached houses built side by side, in a line. The front portico of the Classic Box is absorbed within the rectangular perimeter and the front door recessed to one side. On the same corner, the large peaked roof reaches out over the door and rests on one, two or three classical columns, which vary from square and chunky to turned and slender. The living room has a slanted or square bay with double-hung windows; where space allows, a modest bay also projects into the side yard. Narrow clapboard covers the Rowhouse, windows have wide, flat trim, and a terraced stoop flanks the front steps.

7.6.5 Craftsman Bungalow/California Bungalow (1905-1925)

A new architectural ideal was being embraced by many Californians in early 20th century--one which valued hand crafts over the machine-made, stained rather than painted wood, and the principle that "nothing is beautiful that is also not functional." One realization of these ideals was the Craftsman bungalow, a house form that was typically one to two stories with gently pitched broad gables, one large gable covering the main portion of the house and often a second, lower gable, covering a porch. Equally important was the interior arrangement of space, which eliminated hallways to create open floor plans and incorporated stained woodwork throughout.



Californians were particularly receptive to Craftsman ideas of integrating the house with its natural surroundings, possible, in part, because of the mild California climate and abundance of natural materials. The California Bungalow is a builder’s simplification of the Craftsman bungalow, with stucco replacing wood shingles as the primary exterior finish and a generally simpler level of detail. The feature unique to the California Bungalow is the pair of broad, tapered “elephantine” columns supporting the small gable over the front porch. The bungalow has been referred to as California's first architectural export, variations of which were adapted by communities around the country. Examples can still be seen throughout Martinez and throughout the Bay Area today.

7.6.6 Mission Revival (c. 1915-1939)

Mission Revival, popular all over the country after its introduction in 1893 at the World's Columbian Exposition in Chicago, was particularly attractive to Californians looking for a simpler regional architecture. Romantic ideals of the Spanish-Mexican colonial period prevailed, however unfounded, and seemingly every California town erected a red-tile, white stucco Mission Revival building, often with neo-Moorish towers and round arches. Instead of adhering to authentic early-19th century colonial California examples, Mission Revival style was based more on Mediterranean traditions. These architectural motifs experienced renewed popularity throughout California, and the entire country, from the late 1910s through the 1930s as the Spanish Colonial Revival.



In its simplest form, Spanish styling is characterized by white (usually) stucco exteriors and red tile roofs, with an occasional arched opening. More elaborate examples incorporated grates and grilles of wood, wrought iron or plaster; extensive use of terra cotta and tile; and balconies and patios integrated into plans. Asymmetric massing uses features such as stair towers, projecting planes set off by corbelling, and a variety of window shapes and types.

7.7 AREA -SPECIFIC DESIGN STANDARDS AND GUIDELINES

These standards are in addition to the general standards for new development contained in Chapter 10 of this Plan. Refer to Chapter 10 for general standards and guidelines that apply to the entire Downtown Specific Plan area and Section 3.5 for standards and guidelines for all residential structures in the Downtown Specific Plan Area.

7.7.1 Downtown Neighborhood Character Defining Statement

The character of the Downtown Neighborhood is defined by its existing historic residential buildings. The overall look of the area should remain that of a historic residential neighborhood. More contemporary construction that does not conform to this vision should not be approved.. This is primarily an area for residential uses, including semi- and/or fully-attached single-family homes, , and small multifamily structures. The scale of small lot residential development is important, and the consolidation of individual lots to create larger projects is not in character with this district. Existing buildings should be retained, adapted and reused for residential or other permitted land uses where the building is structurally sound and not in a state of total disrepair. New construction, where warranted, should respect and complement the district’s historic residences in design, scale and placement.

7.7.2 Site Planning

Building Orientation

- a) Buildings should have a traditional residential orientation to the street.
- b) In new buildings, the first floor building levels should be set slightly above the sidewalk level (up to 5 feet) to create a sense of transition from the public space to the private realm.
- c) Privacy between residences should be preserved by offsetting windows from existing windows on the adjacent properties.

Vehicular Access and Parking

- a) Access should be via residential driveways with parking situated toward the rear and side of the lot. No parking is allowed in front of the building.
- b) The width of driveways should not exceed 12 feet, while providing adequate room to maneuver vehicles.
- c) Hollywood drives (middle planting strips) and open pavers are encouraged for residential driveways.

Pedestrian Access

- a) Front entries for residential uses should be clearly identified by elements such as porches or stoops.
- b) Entry walks from the sidewalk to the front door should reflect the residential character of the district. The width of entry walks should not exceed five feet.

Site Furniture

As the primary character of the Downtown Neighborhood is residential, outdoor pedestrian amenities such as residential type seating should be provided in private open spaces such as front porches and rear yards.

7.7.3 Architecture

Style

- a) Existing buildings should be restored to and maintained at their original appearance.
- b) New buildings should use a consistent architectural style for the structure and all elements relating to it, including trellises, carports, roof forms, windows and detailing.
- c) While specific architectural styles are not dictated, several styles predominate in the Downtown Neighborhood District (see Section 7.6). These styles create the architectural vernacular of the District and should be reflected in the architecture of new projects.
- d) The rehabilitation of an older building should work toward restoring the original architecture of the building, rather than covering it over with a new style.
- e) An addition to an existing building should be designed to reflect and blend with the existing design of the structure.
- f) The design of auxiliary structures (detached garages, sheds, etc.) should be architecturally similar to the main structure.
- g) Porches and roofs for a new building should be compatible with the existing patterns in the neighborhood.

Scale

- a) New buildings should respect the overall massing scale of the neighborhood.
- b) Long blank walls should be avoided.

Detailing

- a) New detailing on existing buildings should be accurate to the original detailing of the building.
- b) New buildings should use detailing reflective of the signature buildings in the district.

Roof Design

The typical roof in this area should be a pitched design reflective of nearby residences.

Porches

Porches define a semipublic area that transitions between the public street and the private interior.

- a) Existing porches should be preserved.
- b) Adding new porches to historic structures or enclosing existing porches is strongly discouraged.
- c) Elevated porches are strongly encouraged in new residential structures.
- d) Porches should be an integral element of the building design and not appear added on. Elements that should be consistent between the porch and the main structure include roof slope and architectural details such as columns, balusters, balustrade and brackets.

Windows

- a) All windows on a building should be related in operating type, proportion and trim. Unifying elements such as common headers and sills are encouraged.
- b) Existing, historically correct windows should be preserved in place.
- c) Aluminum sliding windows are not in keeping with the area’s character, and their replacement with windows more in keeping with the area’s historic character, such as wooden double-hung windows, is encouraged.

Colors and Materials

- a) Predominant materials should be stucco, wood siding, shingle roofing, and wood framed windows and doors.
- b) Colors should be appropriate to the style and period of the building. For example, Craftsman styles use muted earth tones, while Victorian styles use brighter, more contrasting colors.

7.7.4 Landscaping

Plant Types

Plant types should be typical of residential plantings, with limited lawn in the front yard, foundation shrubbery, and limited use of small-scale trees as accents.

Scale

Pedestrian scale plantings should predominate, with larger plantings used as accents.

Relationship to Development

- a) Plantings should be arranged to frame the architecture, provide a green carpet between the street and the building, and soften the view of the building foundation.
- b) The public parkway along the street should be landscaped.
- c) Pathways, pergolas and trellises that are in character with the architectural style of the house are encouraged to add shade and interest.

Hardscape

- a) Paving should be on a small scale and limited to walkways, driveways, and rear yard parking areas.
- b) Varied paving textures and/or elevation changes should be used to define entrances, pedestrian areas, and crosswalks.

**ADDITIONAL RESOURCES FOR REHABILITATION
OF HISTORIC RESIDENTIAL PROPERTIES**

McAlester, Virginia and Lee. *A Field Guide to American Houses* (New York, Alfred A. Knopf, 1988).

Prentice, Helaine Kaplan and Blair Prentice. *Rehab Right: How to Realize the Full Value of Your Old House* (Berkeley, Ten Speed Press, 1986).

Chapter 8 – Grandview

This chapter defines the land uses, development standards and design standards and guidelines for the Grandview area. This chapter is organized as follows:

- 8 Grandview
 - 8.1 Purpose
 - 8.2 Uses
 - 8.3 Density
 - 8.4 Zone Districts
 - 8.5 Development Standards
 - 8.6 Design Standards and Guidelines

8.1 PURPOSE

The purpose of the Grandview area is to provide for a variety of single-family housing types and secondary units, with limited multiple residential in an environment that is predominantly single-family residential.

8.2 USES

The uses in the Grandview area shall be those allowed in the underlying zone districts of R-6.0, R-3.5 and R-2.5.

8.3 DENSITY

The allowable density range in this area shall be 7 to 17 units per acre (equivalent to R-6.0 to R-2.5 zoning). The density for an individual new development shall be in accordance with the applicable underlying zone district.

8.4 ZONE DISTRICTS

The existing zone districts of R-6.0, R-3.5, and R-2.5 shall remain on the zoning map within the Grandview area. Opportunity Site #31 shall be rezoned to R-6.0, and Highland Park shall be rezoned to RF.

8.5 DEVELOPMENT STANDARDS

All new development in the Grandview area shall be in accordance with the standards for new residential development (height, setbacks, coverage etc.) contained in the applicable zone underlying zone district, subject to the design guidelines of this Plan.

In addition, proposals for new construction on sites with areas of 10% or greater slopes must comply with the City’s Hillside Development Regulations, which are contained in

Chapter 22.33 of the Zoning Code. For such properties in the Grandview area, the allowable density shall be calculated according to Table No. 1 in Section 22.33.020 of the Zoning Code, using the applicable underlying zone district as a basis for the allowable density calculation.

8.6 DESIGN STANDARDS AND GUIDELINES

These standards are in addition to the design standards for new residential development contained in Chapter 10 of this Plan and the City’s Hillside Development Regulations.

8.6.1 Character Defining Statement

The Grandview area is the residential neighborhood to the east of the Civic Area, extending up the slope to the Shell Martinez Refinery. At the top of the hill, Highland Park provides recreational facilities and a neighborhood gathering space. Many homes are situated on relatively steep slopes and enjoy dramatic views of Downtown, the hills to the west and the Carquinez Strait, as well as proximity to Downtown and the Waterfront. Streets are fairly narrow and winding, and there are numerous dead-end streets. The majority of the homes in the Grandview area were built from about 1910 to 1940, although additional houses continued to be built on the remaining lots following World War II. Scattered among the single-family homes are several duplexes and even a few small multi-unit buildings. To maintain the area’s character, existing homes should be maintained and improved, and secondary units may be added where sufficient space exists. New construction should be in keeping with the scale and massing of existing residences, although it may reflect more contemporary architecture.

8.6.2 Site Design Standards

Building Orientation

- a) Buildings should have a traditional residential orientation to the street.
- b) Windows shall be offset from windows on adjacent structures wherever possible.

Vehicular Access and Parking

- a) Access should be via residential driveways with parking situated toward the rear and side of the lot.
- b) The width of driveways should not exceed 12 feet, while providing adequate room to maneuver vehicles.
- c) Hollywood drives (middle planting strips) and open pavers are encouraged for residential driveways.
- d) Shared driveways are encouraged to reduce the number of curb cuts and increase the amount of on-street parking.

Pedestrian Access

- a) Front entries for residential uses should be clearly identified by elements such as porches or stoops.

- b) Entry walks from the sidewalk to the front door should reflect the residential character of the district. The width of entry walks should not exceed five feet.
- c) Sidewalks should be provided on any new streets created in the district. Where practical and in coordination with development projects, sidewalks should be added to existing streets where reviewed and approved by the City Engineer.

8.6.3 Architecture

Style

- a) A variety of architectural styles are appropriate in this district.
- b) An addition to an existing building should be designed to reflect and blend with the existing design of the structure.
- c) The design of auxiliary structures (detached garages, sheds, etc.) should be architecturally similar to the main structure.
- d) Porches and roofs for a new building should be compatible with the existing patterns in the neighborhood.

Scale

- a) New buildings should respect the overall massing scale of the neighborhood.
- b) Long blank walls should be avoided.
- c) New buildings should have a traditional residential style, reminiscent of other residences in the area.

Roof Design

The typical roof in this area should be of a pitched design reflective of nearby residences.

Porches

Porches define a semipublic area that transitions between the public street and the private interior.

- a) Existing porches should be preserved.
- b) Enclosing porches or adding new porches to historic structures is strongly discouraged.
- c) Elevated porches (up to 5 feet above adjacent grade) are strongly encouraged in new residential structures.
- d) Porches should be an integral element of the building design and not appear added on. Elements that should be consistent between the porch and the main structure include roof slope and architectural details such as columns, balusters, balustrade and brackets.

Windows

All windows on a building should be related in operating type, proportion and trim. Unifying elements such as common headers and sills are encouraged.

Colors and Materials

- a) Predominant materials should be stucco, wood siding, shingle roofing, and wood framed windows and doors.
- b) Colors should be appropriate to the style and period of the building. For example, Craftsman styles use muted earth tones, while Victorian styles use brighter, more contrasting colors.

8.6.4 Landscaping

Plant Types

- a) Plant types should be typical of residential plantings, with an emphasis on small lawn areas in the front yard, foundation shrubbery, and limited use of small-scale trees as accents.
- b) Areas of steep slope should be landscaped and maintained with plantings that help to stabilize the soil and reduce the potential for erosion.

Scale

Pedestrian scale plantings should predominate, with larger plantings used as accents.

Relationship to Development

- a) Plantings should be arranged to frame the architecture, provide a green carpet between the street and the building, and soften the view of the building foundation.
- b) The public parkway along the street should be landscaped and maintained.
- c) Pathways, pergolas and trellises that are in character with the architectural style of the house to add shade and interest are encouraged.

Hardscape

- a) Paving should be on a small scale and limited to walkways, driveways, and rear yard parking areas.
- b) Varied paving textures and/or elevation changes should be used to define entrances, pedestrian areas, and crosswalks.

Chapter 9 – Downtown Shoreline

This chapter defines the land uses, development standards and design standards and guidelines for the Downtown Shoreline area. This chapter is organized as follows:

- 9 Downtown Shoreline
 - 9.1 Purpose and Process
 - 9.2 Uses
 - 9.5 Development Standards
 - 9.6 Design Standards and Guidelines

9.1 PURPOSE

The intent of the Downtown Shoreline area is to provide for a variety of residential uses in an environment that is transitioning from industrial to residential uses. This area serves as a transition area between the urbanized portion of the Downtown and the open space of the Martinez Regional Shoreline to the north. The standards and guidelines for this area are intended to protect and enhance the environmentally sensitive areas of the Shoreline, and contribute to the economic revitalization of Downtown, by permitting a sufficient intensity of development to provide an economic incentive for industrial uses to relocate.

Note: this section not adopted; reserved for possible future action and/or amendment

9.1.1 North Shoreline District

The intent of the North Shoreline District (Opportunity Sites #1 and 2) is to provide for a range of possible future uses such as a community center, parking, open space, mixed use area and residential uses. The current Light Industrial zoning shall remain in place.

Neither site will be rezoned from its existing zoning until the Planning Commission has reviewed and approved a development proposal for that site containing the following (in addition to the normal application requirements):

- An emergency response/secondary access plan as specified in the Final EIR
- A noise and vibration study showing that the proposal meets City standards as specified in the Final EIR
- A soils report and engineered foundation plan, peer reviewed by the City’s consultants, showing that the proposed housing units can withstand the potential seismic shaking and liquefaction hazards as specified in the Final EIR

- A site specific study showing that the proposal meets the requirements specified in the Final EIR for protection of the existing wildlife habitat in the adjacent Martinez Regional Shoreline

9.2 USES

The uses in the Shoreline area shall be those allowed in the new Downtown Shoreline zone district, and shall generally consist of the uses allowed in the City’s existing multiple residential zones.

9.5 DEVELOPMENT STANDARDS

9.5.1 General

All new development shall be in accordance with the standards of the new Downtown Shoreline District All new multiple residential development in this District shall be processed concurrently with a subdivision map, so that individual units can be offered for sale, and shall meet the requirements for new condominium units as contained in Chapter 21.54 of the Municipal Code.

Note: this section not adopted; reserved for future action and/or amendment

- 9.5.2 North Downtown Shoreline District
- Because of the area’s proximity to environmentally sensitive areas and protected species habitat in the Martinez Regional Shoreline, the following additional requirements apply to all development in the North Downtown Shoreline District:
- 1) No construction, development, structure, street, alley or landscaping is permitted within 100 feet of any marshlands or creeks within the Martinez Regional Shoreline. This marshland setback area shall be undisturbed and used as a vegetative buffer to the marshland. Marshland setbacks will be necessary along the north, west and the upper eastern property lines of Opportunity Sites 1 and 2 where the property is adjacent to property containing marshlands and other aquatic habitats. The setbacks are to be to the nearest marsh area and if the nearest marsh area is more than 100 feet from the property line then no set back is necessary.
 - 2) Fencing is required along the landward edge of the marshland setbacks or the property boundary of Opportunity Sites 1 and 2 (where the property is more than 100 feet from a wetland edge) to prevent unrestricted access by people, pets, and pest species to the Regional Shoreline. Fencing is necessary along the north and west boundary of Opportunity Site 1 and north, east and west boundaries of Opportunity Site 2. Design of this

fence, and planting in its vicinity, should be carefully coordinated with the East Bay Regional Park District. One fence design shown to be effective against predatory animals includes tight weave wire mesh walls, an inward curved top, and an underground woven mesh skirt extending 4 feet outwards from the base to prevent burrowing underneath. This type of fence or another design proven equal or better shall be installed in the areas described above.

- 3) Exterior lighting located adjacent to the Martinez Regional Shoreline within Opportunity Sites 1 and 2 must be shielded to prevent the spill of light onto marshlands and other natural habitats.

9.5.3 Maximum Height

The maximum building height in the Downtown Shoreline area shall be 40 feet, or three stories, for development approved at an R-1.25 density, and 30' or two stories, for development approved at a R-2.5 density. In some areas, such as transition areas near existing single family residential areas, a two-story maximum height may be determined to be appropriate by the Planning Commission. The Planning Commission may approve taller buildings by use permit.

9.5.4 Density

The basic allowable density shall be 17 units per acre, equivalent to R-2.5 density. The Planning Commission may approve a higher density, up to a maximum of 35 units per acre, by use permit.

For the portion of the Downtown Shoreline District south of the railroad tracks (Opportunity Sites #3, 4 and 5), in order to approve a density above the lower end of the density range, the Planning Commission would need to find that in addition to meeting the above minimum requirements, the proposal is superior in terms of two or more of the following criteria:

- Assembling all or most of the contiguous parcels into one project, and designing the project as a new neighborhood
- Design and appearance
- Minimizing impacts on adjacent public lands
- Providing onsite amenities for the future residents
- Preserving or creating view corridors from public streets such as Talbart, Buckley, Marina Vista, Carquinez Scenic Drive, Castro and Berrellesa.

- Utilizing green building practices to the maximum extent possible
- Providing a variety of housing types, including detached single family residential, where feasible, as a transition in areas near existing single family neighborhoods.
- Providing a new public street system that improves access to the Regional Shoreline and Alhambra Creek, potentially by extending Alhambra Avenue along the creek, and vacating Berrellesa.

In order to approve a density at or near the upper end of the density range, the Planning Commission would need to find that the proposal is superior in terms of all or almost all of the above criteria.

Note: this section not adopted; reserved for future action and/or amendment.

For the North Downtown Shoreline District (Opportunity Sites #1 and 2), in order to approve a residential density above the lower end of the density range, the Planning Commission would need to find that in addition to meeting the above minimum requirements, the proposal is superior in terms of:

- Design and appearance
- Minimizing impacts on adjacent public lands
- Providing onsite amenities for the future residents
- Preserving or creating view corridors from public streets such as Estudillo, Castro and Berrellesa.
- Utilizing green building practices to the maximum extent possible

9.6 DESIGN STANDARDS AND GUIDELINES

These standards are in addition to the standards for new development contained in Chapter 10 of this Plan.

9.6.1 Character Defining Statement

The character of the Downtown Shoreline area is defined by its proximity to Downtown residential neighborhoods to the south and the Martinez Regional Shoreline to the north. This is primarily a district for residential uses, including semi- and/or fully-attached single-family homes, live-work uses, and small multifamily structures. New development should be planned to create views of the Shoreline from Downtown where possible.

Large industrial uses are encouraged to relocate out of the District, but smaller, self-contained service commercial uses may coexist with existing and new residential uses.

9.6.2 Site Planning

Building Orientation

- a) Buildings should have a traditional residential orientation to the street.
- b) In new buildings, the first floor building levels should be set slightly above the sidewalk level (up to four feet above grade) to create a sense of transition from the public space to the private realm. Accessible routes may be provided from rear alleys.
- c) Privacy between residences should be preserved by offsetting windows from windows in adjacent buildings.
- d) New streets should be oriented where possible to provide views into the waterfront.

9.6.3 Architecture

Style

- a) New buildings should have a traditional residential style, reminiscent of existing residences in the adjacent Downtown Neighborhood District.
- b) A consistent architectural style should be used for a building and the elements that relate to it, such as trellises, carports, roof forms, windows and detailing. While specific architectural styles are not dictated, several styles predominate in the Downtown Neighborhood District and the other residential parts of Downtown Martinez and should provide inspiration to help maintain Martinez' unique character. Styles need not be replicated literally, but should be clearly reflected in a proposed project.
- c) For buildings with more than six residential units, or projects with more than two residential buildings, design shall be varied, not uniform or monotonous.

Scale

- a) New buildings should respect the overall massing scale of the neighborhood.
- b) Long blank walls should be avoided.

Roof Design

The typical roof in this area should be of a pitched design reflective of nearby residences.

Windows

All windows on a building should be related in operating type, proportion and trim. Unifying elements such as common headers and sills are encouraged.

Colors and Materials

- a) The predominant materials should be stucco, wood siding, shingle roofing, and wood framed windows and doors.
- b) Colors should be appropriate to the style of the building. For example, Craftsman styles use muted earth tones, while Victorian styles use brighter, more contrasting colors.

9.6.4 Landscaping

Plant Types

- a) Plant types should be typical of residential plantings, with foundation shrubbery and limited use of small-scale trees as accents.

Scale

Pedestrian scale plantings should predominate, with larger plantings used as accents.

Relationship to Development

- a) Plantings should be arranged to frame the architecture, provide a green carpet between the street and the building, and soften the view of the building foundation.
- b) The public parkway along the street should be landscaped and maintained.
- c) Pathways, pergolas and trellises that are in character with the architectural style of the building to add shade and interest are encouraged.
- d) Landscaped areas should be regularly maintained to prevent deterioration of the property.

Hardscape

- a) Paving should be on a small scale and limited to walkways, driveways, and rear yard parking areas.
- b) Varied paving textures and/or elevation changes should be used to define entrances, pedestrian areas, and crosswalks.

Chapter 10 – General Design Standards and Guidelines

This chapter defines general design standards and guidelines for the Downtown Specific Plan area. This chapter is organized as follows:

- 10 General Design Standards and Guidelines
 - 10.1 Applicability
 - 10.2 Purpose
 - 10.3 Site Design Standards
 - 10.4 Architectural Design Standards
 - 10.5 Additional Standards for Residential Development
 - 10.6 Additional Standards for Live/Work Units

10.1 APPLICABILITY

The provisions of this chapter apply to all development in the Downtown Specific Plan area.

The design standards and guidelines in this chapter apply in addition to the Area-specific development and design standards and guidelines in the preceding chapters and in addition to adopted City policies and Zoning Code requirements for landscaping, parking and trash enclosures.

10.2 PURPOSE

The development and design standards and guidelines for Downtown Martinez are intended to provide property owners, merchants and their designers with basic development and design criteria that are intended to reinforce the desired building and area character.

The goals of the design standards and guidelines are as follows:

- a) Provide basic design requirements for all buildings in the Downtown, promoting design creativity and variation while ensuring consistency in building scale, proportion and pedestrian orientation.
- b) Establish clear and usable standards, guidelines and criteria for development.
- c) Protect and enhance historic buildings and utilize historical building forms and styles to create future buildings.

10.3 SITE DESIGN STANDARDS AND GUIDELINES

New development in the Downtown Specific Plan area should be compatible with surrounding development and historic structures, as well as pedestrian-friendly. The street environment should also enhance pedestrian safety and comfort through a sensitive application of street furnishings, benches, enriched paving, and lighting. This will strengthen the historic character while simultaneously providing a functional environment.

10.3.1 Additions, rehabilitation and new structures

- a) New structures shall be sited in a manner compatible with surrounding development and with the façade facing the public street designed in a manner that enhances the pedestrian environment.
- b) Additions shall be compatible with the existing building in scale, materials, and design.
- c) Wherever possible, mature trees shall be preserved or relocated on site.
- d) New structures and parking areas shall enhance existing pedestrian connections to existing outdoor pedestrian spaces, such as sidewalks and plazas, and create new connections where none exist.

10.3.2 Building Access

- a) Main entries to buildings shall be clearly demarcated, visible and accessible from the street and/or pedestrian corridors. Secondary entries may be from parking areas. Entries shall not occupy more than one third of the ground floor façade.
- b) Retail entrances shall not be recessed more than five feet and should be located no more than 50 feet apart.
- c) Primary entrances to corner building shall be at corners wherever possible.

10.3.3 Parking and Site Access

All parking and service/loading areas shall be developed per the requirements of Section 22.36 of the Zoning Code. In addition:

- a) If provided, on-site parking shall be consolidated in one area rather than wrapping around the building.
- b) Driveways shall be kept to a minimum and shared site access is encouraged.
- c) All service/loading areas shall be screened from view from public streets and walkways and removed from pedestrian oriented areas. These screens shall be located at the setback line to maintain continuity of setback patterns within the district.
- d) All parking areas shall be landscaped per the requirements of Section 22.36.080 of the Zoning Code.

10.3.4 Trash and Utility Enclosures

- a) Trash storage areas and utility structures should be located to the rear of the site and, where possible, screened from view from public streets and walkways and removed from pedestrian oriented areas.
- b) Colors and materials used to enclose these elements should be compatible with all other buildings on site.

10.3.5 Mechanical Equipment Screening

- a) All roof-mounted mechanical equipment should be screened from view of pedestrians and users of adjacent buildings by either a building parapet or mechanical penthouse.
- b) The parapet should be designed as an integral part of the building. Mechanical penthouses shall be designed and painted to blend in with their visual background.
- c) Wooden screens should not be permitted.

10.3.6 Site Landscaping

- a) In general, landscaping should be used to soften large building walls and parking areas and enhance building entrances.
- b) Site area devoted to landscaping should be greater than 5% of the overall parcel area, except as noted within the Downtown Core.
- c) The use of flowering vines is encouraged along fence lines, perimeter walls and blank building elevations.
- d) Both deciduous and evergreen trees should be planted to provide variety in textures, color and form.
- e) Canopy trees to provide shade are encouraged in parking lots and front setback areas.
- f) Colorful accent plants should be used to enhance entrances and add interest at special locations. These may be provided in pots, planter boxes, and hanging baskets as well as ground plantings.
- g) Landscaping in and around parking areas should not exceed three feet in height, with the exception of trees.
- h) Stretches of screening (landscaping, walls, or hedges) longer than 45 feet should include accent points using a different element or plant material or combination of the two to create a visual break in the screening material
- i) Hedges and other landscape screening materials should consist of evergreen plant materials.
- j) In addition to the standards for water conservation contained in Chapter 22.35 of the Zoning Code, general criteria for plant material selection also include compatibility with the building architecture and low maintenance needs.

10.4 ARCHITECTURAL DESIGN STANDARDS AND GUIDELINES

New development in Downtown Martinez should express historical architectural characteristics. New projects need not be literal replicas of existing buildings but should generally have a traditional appearance. Designers should assimilate and reinterpret building massing, materials and details from existing structures dating from Downtown's period of greatest historical significance, roughly 1850-1930. This promotes a variety of building styles, which contributes to the interest and vitality of Downtown, while maintaining traditional building forms that will reinforce the setting of Downtown's many historic buildings.

10.4.1 Massing, Form and Scale (All new structures, including additions)

- a) The size and mass of new structures, including additions, should be in relation to surrounding structures.
- b) Architectural features should reflect or be compatible with the character-defining architectural features of surrounding structures or with the predominant architectural styles within the Area.
- c) To create visual interest, where appropriate, varied roof or parapet heights and/or shifts in wall plane should be used.
- d) Building corners may be emphasized by the use of architectural elements or entries.
- e) Building articulation can be accomplished with the placement of windows and entries, volume changes, significant color and material changes, and the creation of shadow textures with trellises and overhangs.

10.4.2 Building Façade and Elevation Design (All new structures, including additions)

- a) Building walls that are visible from a public street, major pedestrian corridor, or public open space should include architectural features such as windows, arcades, canopies, and trim to create visual interest.
- b) Street-facing building façades should not have a section of blank wall exceeding 30 linear feet without being interrupted by a window or entry.
- c) At least 60 percent of the linear length of street-facing non-residential façades, on each story, should contain windows, doors, or arcades. Clerestory windows or other windows with sills more than four feet above the exterior grade do not count toward the 60 percent requirement.
- d) The appearance of building mass may be reduced through the use of arcades, courtyards, pergolas and stepping stories back above the ground level.

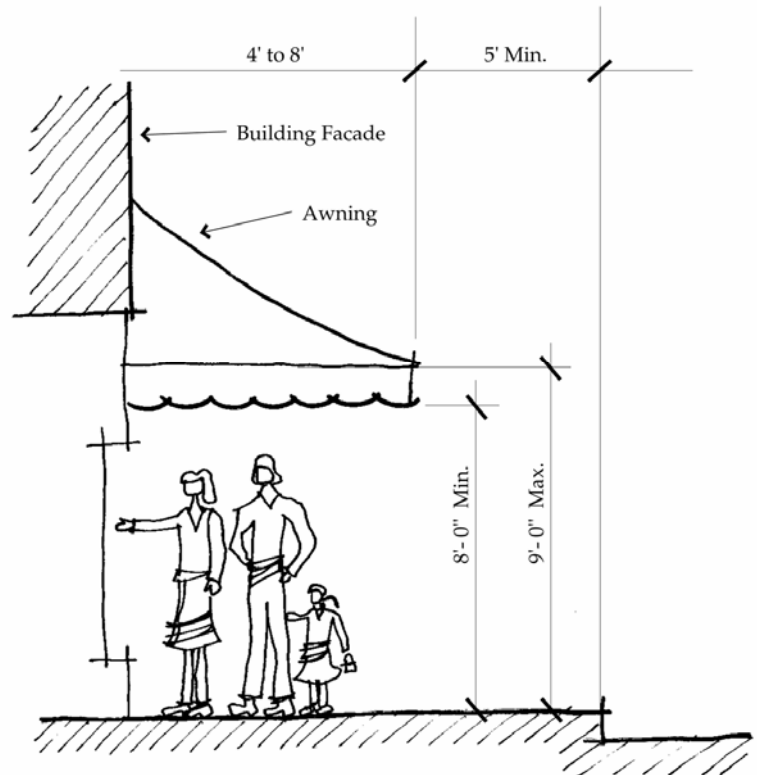
- e) Windows and doors should be proportioned to and integrated with the façade modulation. Establish clear vertical and/or horizontal hierarchy and patterns in the placement of openings and assemblies.
- f) Details or elements should be integral to the design and reflect the structural or material integrity of the building, rather than appearing added on.
- g) Color and material changes should be used to add interest and reduce a building's apparent scale.

10.4.3 Architectural Elements

- a) Retail storefronts should have large display windows oriented toward the public street or major pedestrian corridors and a simple entry door centrally located on the building façade.
- b) Retail storefronts should be broken up by architectural features approximately every 25 feet.
- c) Display windows should provide a clear view of store merchandise and a view into the business interior. To achieve this purpose, at least two-thirds of the window surface should remain clear and free of obstructions. This zone should be between four and eight feet from the base of the façade. Ground floor wall sections without windows should not be more than five feet in width for retail uses.
- d) Display windows should consist of a single pane of glass. When required to be divided into smaller sections, windows should have clear silicone vertical joints or minimally sized glazing bars or muntins, used to enhance the architectural style.
- e) A bulkhead between 15 and 24 inches in height should be provided at the base of the storefront display window. However, new storefront buildings may use floor-to-ceiling display windows if the design is compatible with surrounding architecture and appropriate to the area.
- f) Where pilasters will enhance the architectural style of the building, they should be used on the façade to create a visual frame. Pilasters may extend the full height of the building or be limited to the storefront level.
- g) Transom windows may be provided above the display windows. The transom window height depends on the overall ceiling height and ranges from eighteen inches to three feet. Transom windows may have clear, tinted or etched glass.
- h) Awnings should not cover the storefront piers or pilasters and should be divided into sections to reflect the major vertical division of the façade. The awning should be mounted such that its valence is between eight and nine feet above the sidewalk with a projection of between four and eight feet from the building face, but no closer than five feet to the street curb. Retractable awnings are encouraged, but barrel-shaped awnings are discouraged.
- i) The upper level windows should be symmetrically arranged. The number of windows should be based on the storefront modulation at the street level. The windows may be combined into pairs, triples or bands. These windows should be articulated with delineated sills, lintels, or frames so as to create shadow lines.

- j) Building corners should be enhanced with higher massing and entries.
- k) The cornice should enhance the architectural style of the building. A brick-front building may have a corbelled cornice. A plaster front building may have a stone still or terra cotta or ceramic tile detail at the parapet line.
- l) Windows should be clear glazing. Reflective or tinted glass is not permitted.
- m) New buildings may have flat or sloping roofs, depending on which is most compatible with the architectural style of the building and others in the area. Parapets should appear integrated with the building and must always include a cap and corner detail to create a shadow line. Mansard roofs are discouraged.
- n) Towers, bay windows and cupolas are dramatic features of Victorian and Edwardian residential architecture. Appropriate contemporary expression of these elements is encouraged in new buildings. Particular care should be taken in using these elements in terms of scale, proportion, and architectural compatibility with the rest of the building.

GUIDELINES FOR AWNINGS



10.4.4 Colors

- a) The colors chosen should accentuate the architectural details of the building and be consistent with the architectural style.
- b) In general, the building should incorporate a minimum of three colors: a base color on the majority of the building, which is often the lightest color; a major trim color to accentuate certain elements such as the cornice, window frames, and storefront bulkhead; and a minor trim color for window sashes and doors. In addition, an accent color may be used to highlight small details and should contrast with the base and trim colors. Accent colors should be used sparingly.
- c) Stone and masonry should not be painted. Other colors on the façade should be chosen to complement the colors of exposed materials.

- d) Colors for graphics, such as signs, should be related to the colors used on the building. The accent, major or minor trim colors may be used for signs.
- e) Awning colors should be compatible with the building colors. Darker, saturated colors that pick up the highlights of the building colors are preferred. Simple stripes or tweeds are allowed.
- f) The maximum number of colors, including both building and signage colors, should not exceed six.

10.4.7 Site Lighting

- a) Lighting fixtures should be compatible with the architectural character of the project and surrounding area. While some nondescript fixtures may be appropriate, significant use should be made of fixtures that have architectural value and accent the building and site.
- b) All lighting fixtures, including spotlights, electrical reflectors, and other means of illuminating signs, structures, landscaping, parking, loading, and similar areas, should be focused, directed, and arranged to prevent horizontal glare or direct illumination on adjoining property or streets. Lighting shall be directed toward walls and landscaping to avoid shining light up into the sky or onto an adjacent property. No lamp or lens may be visible. No mercury vapor utility yard lights or other light fixtures with high intensity discharge lamps or bulbs which are not designed to limit or control light direction, or which do not shield the light source from view of adjacent properties, should be permitted.
- c) Indirect illumination of neighboring residential properties or uses by any on-site lighting should not exceed 0.5 foot candles at the property line, as measured from the adjacent grade to a height of 14 feet.

10.4.8 Architectural Lighting

- a) Architectural - Accent lighting of architectural features is encouraged to highlight building massing and enhance the pedestrian environment. Accent lighting should not be a source of glare, reflected glare, or excessive light, especially when viewed from residences, streets, walkways, or open spaces. Neon lighting does not qualify as, and is not permitted for, accent lighting.
- b) Building entries - Building entries with high activity levels shall be illuminated. Appropriate treatments include: accentuating building entries with light, allowing the building interior light to glow through glazing, or using decorative lighting fixtures to announce entries.
- c) Service areas - Building-mounted downlight fixtures, in combination with pole fixtures, are preferred for the illumination of building service areas. Such fixtures do not cause glare or light leakage beyond the service areas.
- d) Landscaping and furnishing - Uplighting is recommended for all landscaping and furnishings (in both public and private areas) that require accenting (such as specimen trees, shrubs, and sculptural features).

- e) Landscaped walkways and plazas- Landscaped walkways and other pedestrian paths should be lit by pole or bollard type fixtures that are human scale, typically not to exceed 16 feet or 4 feet in height, respectively. Lighting bollards should have a colored metal finish and a diameter of approximately eight inches. Around the light source near the top of the bollard, horizontal louvers should be used to stylistically complement the luminaries and direct light downward.

10.4.9 Security Grilles

- a) Visible security grilles are prohibited.
- b) Existing security grilles in the Downtown Core District should be removed.

10.4.10 Sidewalk Dining

- a) Sidewalk dining is encouraged to bring life and vitality to streets in the Downtown Core. On Pedestrian-Priority Streets, with a valid sidewalk encroachment permit, portable tables and chairs may be placed on the sidewalk in front of any business serving food or beverages for on-site consumption, provided that an unobstructed pedestrian path of at least 5 feet in width is maintained and that tables and chairs are removed at the end of each business day.
- b) Alcohol may not be served or consumed within the sidewalk encroachment area without prior approval of the Alcoholic Beverage Control Board (ABC) and compliance with City and ABC requirements for a delineated physical barrier enclosing the encroachment area.
- c) A sidewalk encroachment permit is required for the erection of any permanent structures in the public right of way, or for the use, in a public right of way, of tables, chairs, umbrellas, fences, barriers, portable heaters or other appurtenances for sidewalk dining.
- d) In new construction or reconstruction, the design of dedicated outdoor seating areas that do not encroach upon the public right of way is encouraged. Such outdoor eating areas shall be open to and visible from an adjacent street, courtyard, creek or park.

10.5 ADDITIONAL STANDARDS FOR RESIDENTIAL DEVELOPMENT

10.5.1 Applicability

The provisions of this section apply to new residential development in the Downtown Specific Plan area. These standards are in addition to the development standards and guidelines of the applicable Specific Plan and zone District.

10.5.2 Purpose

Historically, Downtown Main Streets included a mix of shops, offices and restaurants at the street level, with residences on side streets and upstairs. This pattern created a rich mix of uses and allowed urban activity to carry on well into the evening. In the 20th century, Downtowns were increasingly zoned for one activity only, namely business, and were shut down after six in the evening. In recent years, cities have once again begun to encourage residential uses within their Downtowns and thus tap into the potential of increased economic activity by extending the usability of Downtown resources into the evenings and weekends. Residential uses are encouraged in the Downtown, including multifamily, townhouse and live/work development, along with the rehabilitation and adaptive reuse of historic structures for residential uses or mixed use.

10.5.3 Design Standards

A. Housing Types

A diverse range of housing types is permitted and encouraged within each land use designation. At the same time, some housing types may be inappropriate to certain areas. The housing types allowable in each Specific Plan District, where residential use is otherwise permitted by the applicable district use regulations, are shown in the following table:

Table 10-1. Allowable Residential Types by Plan District
(where otherwise permitted by applicable district use regulations)

Building Type	DISTRICT			
	Downtown Core	Downtown Neighborhood	Downtown Shoreline	Grandview
<i>Single-Family Standard Lot</i>		*	*	*
<i>Single-Family Small Lot</i>		*	*	*
<i>Duplex</i>	*	*	*	*
<i>Green Court</i>			*	
<i>Townhouse/Rowhouse</i>	*	*	*	*
<i>Live/Work Units</i>	*		*	
<i>Apartments/Condominiums (including Mixed-Use)</i>	*	*	*	
<i>Secondary Units</i>		*	*	*

B. Development Standards

The development standards for each residential building type are specified in each zoning District.

C. Building Orientation

- (1) Buildings should have a strong street presence, with public entrances and front doors oriented toward the street or to a public pathway adjacent to open space.
- (2) Privacy between units should be maintained by locating windows away from windows in adjacent units.

D. Vehicular Access and Parking

- (1) All garages should be set back at least 20’ from the front property line to provide an adequate parking apron and should be set back from the residence’s front façade a minimum of 5’.
- (2) For single-family homes and duplexes, front-loading garages should not make up more than 50% of the street-facing width of the house. For townhomes and green court homes, the garage shall be accessed from the rear.
- (3) For multi-family residential, garages should not make up more than 33% of any street-facing façade, and rear-accessed garages or interior parking lots are encouraged.
- (4) If parking is provided within the building footprint, the first floor of the residential units should not occur more than four feet above the finished grade level. Parking may need to be lowered partially or completely below grade. Finished grades of front entrances may be raised by up to four feet to accommodate this arrangement. These parking areas should not be visible from the street.

E. Entrances: Porches and Entries for Single-Family and Attached Single-Family

All front entrance areas should be easily recognizable from the street and include a welcoming architectural feature such as a porch or covered entry. This is critical not only for practical purposes, but because an important element of residential environments is the relationship of the private home to the public street and sidewalk.



All porches should be covered and form an integral feature of the building.

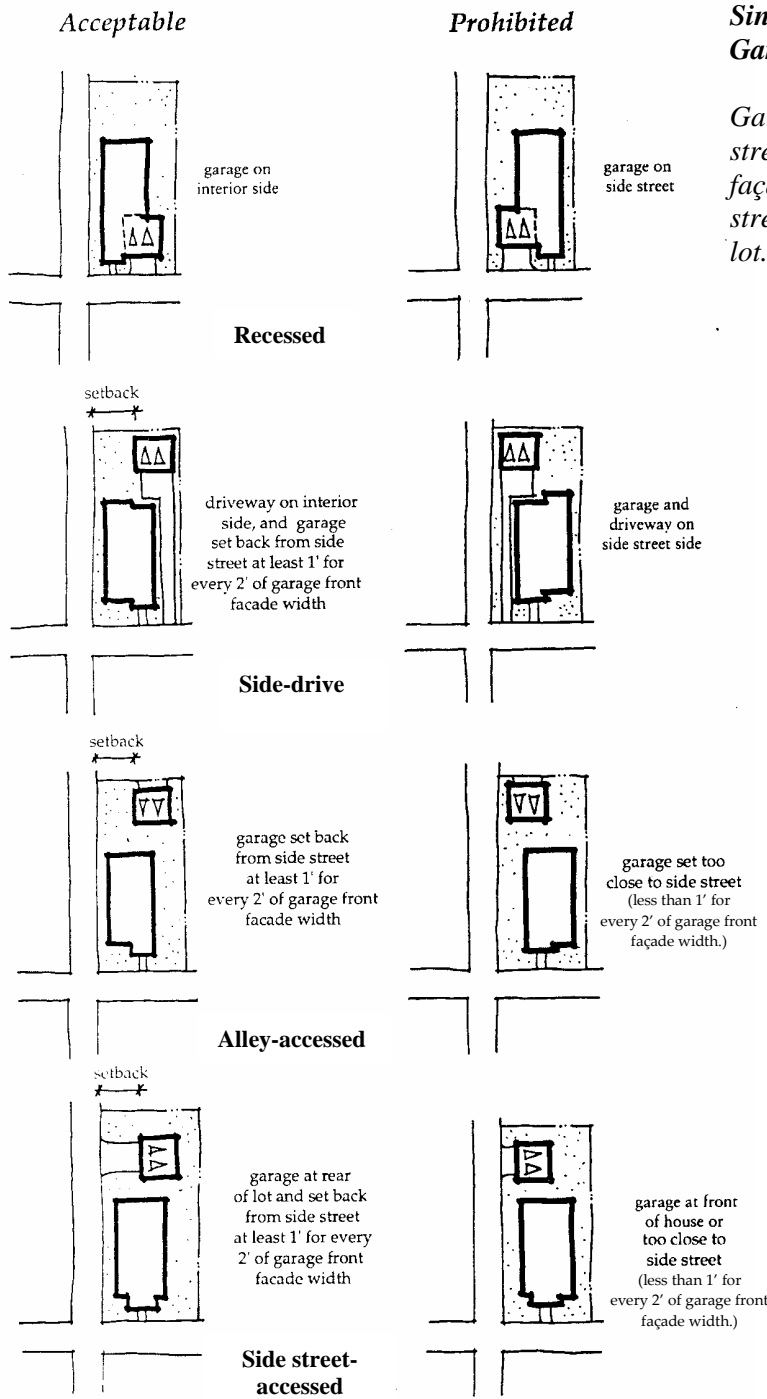
1. *Porches* should be covered and should extend at least 10’ along the front wall of the house, not including the garage face. The minimum depth of the porch should be 6’. Porches may be raised or at ground level. Porch floors should be a hard surface material such as concrete, wood, tile, brick, or cut or flat stone.

Porches should be an integral architectural feature with the main structure. All porches should incorporate railings, either 24” min. ornamental or code height when required by UBC. Porches may extend up to 5 feet into the front setback.

2. *Entries* should be accompanied by a projecting overhead element such as a shed, arch, or gable which provides roof coverage and weather protection. Entries should be a minimum of 6' wide and 4' deep, with floors of a hard surface material such as concrete, wood, tile, brick, or flat or cut stone. Entries should be architecturally integrated with the main structure.



Entries should have a projecting overhead element to provide weather protection.



Single Family Detached and Duplex Garages: Acceptable Corner Locations.

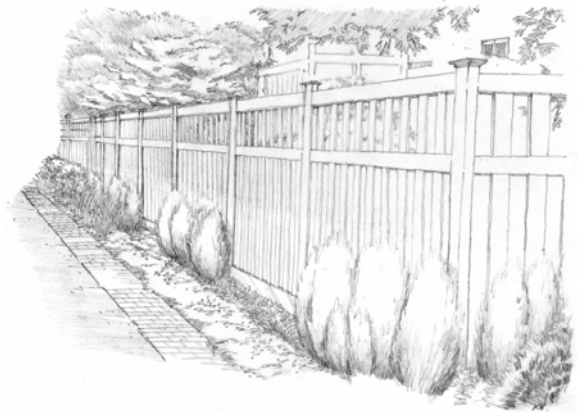
Garages should be set back from the side street by at least 1' for every 2' of garage façade width. Driveways from the front street should be on the interior side of the lot.

F. Fencing

Fences help to define the edges of yards and give privacy to side and rear yards. They are considered background elements that help to highlight landscaping and architecture.

The following guidelines apply to new fencing throughout the Specific Plan area:

1. **Locations:** Side Yard – permitted in side yard setback, except within 5 feet of front building façade. Side Yard on Corner Lot – 10 feet minimum setback from side street right-of-way; one-half of lot depth minimum setback from front street right of way. Alley Fences – minimum 2 feet setback from rear property line.
2. **Height:** 6 feet maximum at rear and side yards; 3 feet six inches maximum in front yard.
3. **Acceptable Materials:** Metal, Wood, Plastic-wood composite (e.g. Trex), Masonry (including veneer). Chain link fencing should not be permitted.
4. **Design:** When a fence is taller than 48 inches, the top 18-24 inches of the fence facing a public street or alley should have a transparency of 30% or greater.



G. Open Space

The following requirements shall determine the amount of required private and common open space:

1. **New Projects**
 - (a) **Private Usable Open Space:** At least 50% of the dwelling units in a project should include private usable open space, as defined in Section 22.04.560 of the Zoning Code, of a minimum of 50 square feet. Private open space may include porches, balconies, and privately owned front and rear yards. All dwelling units in a project are encouraged to include private usable open space. A rectangle inscribed within each private usable open space should have no dimension less than six feet. At least one exterior side shall be open above the level of railing or fencing. Balcony/ railing enclosures should not be see-through.
 - (b) **Common Usable Open Space:** For each unit in a project that does not provide private usable open space as defined above, at least 25 square feet of common usable open space, as defined in Section 22.04.560 of the Zoning Code, should be provided. Common usable open space may be divided into more than one area; however, each area should be a minimum of 450 square feet and a rectangle inscribed within each should have no dimension less than 20 feet. All required common open space should be suitably improved for its intended purposes and all

lawn and landscaped areas should be provided with a permanent irrigation system to maintain such areas. Common open space may include courtyards, terraces, and roof decks.

2. Conversions of existing buildings:

There shall be no minimum open space standards for conversions of existing buildings; however, every effort shall be made to achieve open space in all of the above categories to the extent feasible for the building being converted.

The following diagrams illustrate some of the standards and guidelines specific to each residential building type:

Alley Accessed Front Recessed Side Drive

Single Family /Garage Placement Options

Green Court Configuration

Alley Access Garages Court-yard
Shared Porch Front Access

Alley Access
Front Access

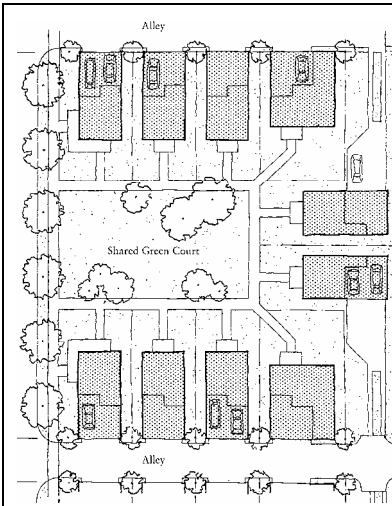
- Minimum 5 foot setback from primary façade to front-loaded garage.
- 16 foot maximum driveway width at street right-of-way.

Duplex

Detached Garages Alley Garage Entries
Back yards
On-street parking
Public street

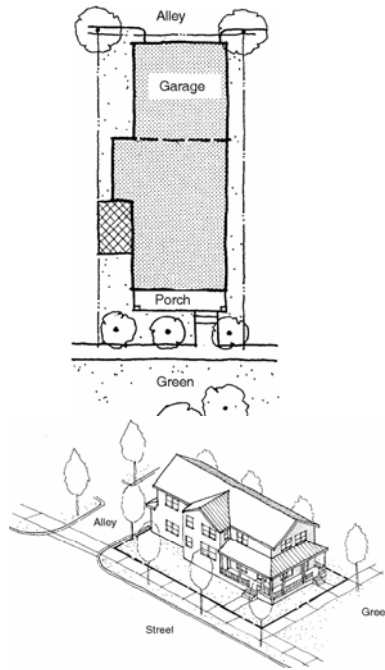
- Garage access from alleys located at rear of lots is required.
- Tandem parking is permitted for up to 50% of units.
- Entry porches are required along 30% of primary façade of each unit or 10' clear, whichever is greater.
- Onsite visitor parking not required for projects of less than ten units.

Townhouse/Rowhouse

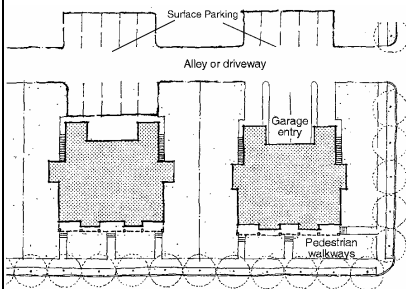


Green Court Configuration

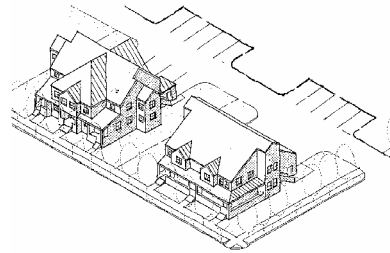
Green Court



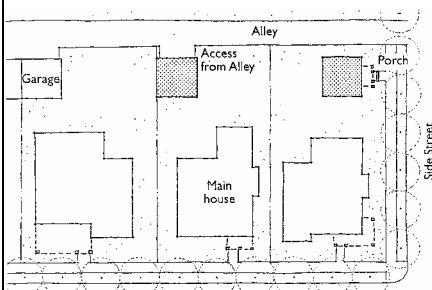
- All houses front onto a shared green space of at least 600 sq. ft. per unit.
- Sidewalks connect front doors directly to street.
- Front porches required, with 5-foot min. setback from green to porch.
- 10 foot min. setback from green to main structure.
- Alley-loaded garages required with 4 foot setback from alley.
- Where porches occur, they should extend at least 10 foot along the front facade



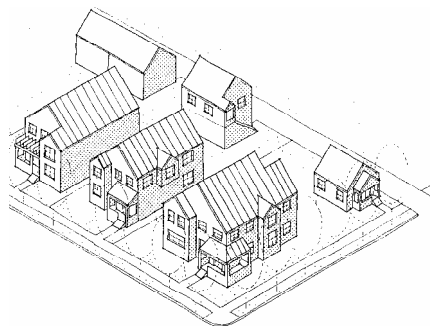
Multi-Family



- Garage access from alley or driveway located at rear of lots is encouraged.
- Encourage pedestrian connections from sidewalks to building entries at public streets
- Projects of ten or more units should provide one guest space per four dwelling units, either off-street or on newly created streets.
- Require entry porches for first floor units which have direct access to public streets.
- Provide balconies for above grade units facing the street.



Secondary Unit



- Secondary Units within rear 1/3 of parcel.
- Secondary Units cannot cover more than 50% of rear yard
- When located on a corner lot the secondary unit should have an entry porch oriented to the side street.
- One on-site parking space required, surface parking allowed.

10.5.4 Architectural Guidelines for Residential Structures

A. Style

The residential character of the individual units should be protected while conforming to the urban character of the applicable Area. Multifamily projects in the Downtown Core Area may have a “Main Street” architectural character, while multifamily projects in the other residential Areas may take their design cues from the nearby historic homes and small multifamily buildings.

B. Scale

- 1) The mass and roof forms of buildings should be varied. In addition to porches, stoops and other entry elements, massing elements such as bay windows, balconies and trellises are encouraged.
- 2) Where considerations of access for people with disabilities allow, the street floor building level should be raised between two and four feet to protect the privacy of ground floor units.
- 3) Façades of multifamily buildings should be divided into shorter segments a maximum of 30 feet in width, to reflect the mass of individual units within the building. This objective can be achieved with varied setbacks, vertical modulation, texture changes on the façade, porches, and balconies.

C. Materials

- 1) New buildings should reflect prevailing architectural styles in Martinez and maintain a high level of craft in construction and materials.
- 2) Exterior finishes should be primarily wood, masonry, and/or stucco.



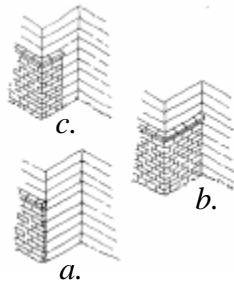
Stucco Finish



Wood Finish

- 3) Material changes should not occur at external corners (“a.” on diagram at right), but may occur at “reverse” or interior corners (“b.” on diagram at right), or as a “return” at least 5 feet from external corners (“c.” on diagram at right.)

- 4) T-1-11 and similar grooved plywoods and pressboards, vinyl or aluminum siding, and vinyl or aluminum trim should not be used.



*Material changes should be at interior corners (b.) or as a “return” at least 5 feet from an exterior corner (c.)
Material changes at exterior corners (a.) are prohibited*

D. Windows

- 1) Provide each primary room with operable windows on at least two sides for balanced natural light and effective cross ventilation.



Windows provide natural light and cross ventilation.



Windows looking onto the street will help make the neighborhood safer by creating more “eyes on the street.”

- 2) Provide at least one major window which looks out onto the street from a living area. Not only will these “eyes on the street” make each neighborhood a safer place, but this connection to the street will help neighborhood interaction.
- 3) Consider locating the windows on the front and back of each building to match its solar orientation. A façade with more windows will work for both south and east orientations. A façade with fewer windows will work for both north and west. South and west facing windows not shaded by roof overhangs could have trellises or awnings. Tailoring window placement to the home’s location creates alternating elevations which vary for practical, ecologically sound reasons.

- 4) Window styles with undivided panes of glass (“single-light windows”) are characteristic of many of the architectural styles prevalent in Martinez and are encouraged. However, some prevalent styles include windows with muntin bars dividing the panes of glass (“divided-light windows”), and manufacturers continue to design ways of achieving the look while still using larger sheets of glass. If simulated muntin bars or snap-in grills are used to create the appearance of divided lights, exterior muntin bars with a raised profile that projects a minimum of one-half inch beyond the glass should be used .

E. Trim

- 1) Trim should be applied consistently around the building. If there is a water-table or sub-fascia in one location on a building, it should occur in all other similar situations on the same building. If there are shutters on one window of a room, there should be shutters on the other windows of that room as well.
- 2) Trim should be appropriately scaled to the size and style of the building. Exceptionally large or overly elaborate trim on a small building can make it look like a cartoon. A large building with overly small and simple trim can look sparse and stingy.
- 3) Trim should be applied three-dimensionally. Horizontal trim bands should wrap outside corners and only terminate at inside corners. Fascia should wrap gracefully from rake to eave.



Trim should be applied three-dimensionally.

F. Roofs

The form of the roof is one of the most memorable and characteristic elements of a residential building. Traditionally, it says much about a building’s style, location, history and construction.

- 1) Keep the overall roof form of each building simple and compact. This is particularly important with smaller buildings. A street of small buildings can feel chaotic if each one has a complex roof.

- 2) Where pitched roofs are used, main roofs shall not be less than a 6/12 pitch, and the pitch of secondary roofs shall not be less than 4/3.
- 3) Roof materials should be appropriate to the style of the building, roof form, and slope. Heavier or more complex roofing materials (tile, concrete tiles) should be placed on simpler roofs - if they are used on complex roofs, they can cause leakage or unnecessary problems. More complex roofs, however, require monolithic, simpler materials (shingles). Allowable materials for roofs include tile, slate, fire-retardant shake, concrete tiles, and composition shingles. Shingles with an architectural grade shadow line, rather than a simple 3-tab, are preferred.



Overhangs and shading devices create visual variety.

G. Garages

All garage doors should be designed to have an attractive appearance. Strong shadow lines should be created around the garage face by recessing the door six inches to a foot behind the adjacent building plane. Another option is to add a trellis that extends at least two feet over the garage face such that it adds strong shadows on the garage door face. Instead of a flat door, multi-panel doors should be used to break down the scale of garage doors.

10.5.5 Landscaping and Site Furniture

Plant Types

- 1) Plantings of shrubs and flowering plants to add variety to the setback areas are encouraged.

- 2) Pathways, pergolas and trellises that are in character with the architectural style of development to add shade and interest are encouraged.

10.5.6 Signage

Style

- 1) Traditional designs that reflect the building architecture are encouraged.
- 2) Signage for multifamily uses should be discreet and subdued.

10.6 ADDITIONAL STANDARDS FOR LIVE/WORK UNITS

10.6.1 Applicability

The provisions of this section apply to live/work units, as defined in Appendix D, Definitions. These standards are in addition to the development standards and guidelines of the applicable Specific Plan area. Refer to Chapter 11 for signage standards and Chapter 12 for parking standards.

10.6.2 Purpose

The intent of this section is to provide for and make feasible the reuse of existing commercial, industrial or residential structures to accommodate live/work opportunities, as well as to provide opportunities for the new development of buildings specifically designed and constructed to provide live/work units.

10.6.3 Floor Area Requirements

The minimum floor area of a live/work unit should be 750 square feet.

10.6.4 Access to Units

Each unit should have access to the living space independent of access to the working space. Access to living space should be provided either directly from the sidewalk or from porches, courtyards, or alcoves, each of which may provide access to up to four units.

10.6.5 Internal Layout

All living space within the live/work unit should be contiguous with and an integral part of the working space, with direct internal access between the two areas.

10.6.6 Occupancy and Employees

At least one of the full-time workers of the live/work units shall reside in the unit. The residential area shall not be rented separately from the working space. The business activity occupying the live/work unit may utilize employees in addition to residents as necessary, subject to the occupancy level allowed by the building code. Working space shall be subject to fire code provisions for commercial occupancies.

10.6.7 Retail Sales

Retail space may be integrated with working space.

10.6.8 Business License

A business license shall be obtained in compliance with the City Code for business activities conducted within the live/work unit.

10.6.9 Zoning

Live-work units shall be added to the new zone district of Downtown Shoreline as a conditional use, and to the amended CC zone district as a permitted use.

Chapter 11 – Signage

This Chapter recommends standards for permitted and prohibited signage for the Downtown Specific Plan area. The intent of this Chapter is to preserve and enhance the appearance and safety of Downtown Martinez by encouraging signage that communicates clearly to Downtown visitors, residents and employees. This Chapter is organized as follows:

- 11 Signage Standards
 - 11.1 Applicability
 - 11.2 Signs Permitted in All Areas
 - 11.3 Prohibited Signs
 - 11.4 Area-Specific Design Standards

11.1 APPLICABILITY

This Chapter is intended to supplement the existing City regulations for signs, which are contained in Chapter 16.20 of the City Code. Exceptions to the existing regulations are noted below and will supersede the conflicting section in the City Code, when adopted as amendments to the City Sign Ordinance.

11.2 SIGNS PERMITTED IN ALL AREAS

The following types of signs are permitted in all Downtown Specific Plan areas. Unless otherwise noted, the City Code standards shall apply.

11.2.1 Permitted for All Uses

- a) Temporary signs
- b) Directional signs
- c) Flags
- d) Political Signs
- e) Occupancy signs
- f) Real estate signs
- g) Future use signs
- h) Noncommercial signs
- i) Signs for nonconforming uses

11.2.2 Residential Uses

For residential development requiring Design Review and consisting of more than 10 dwelling units, one on-premises wall sign or monument sign not exceeding twelve square feet in area per display face, is permitted. Monument signs may not exceed four feet in overall height.

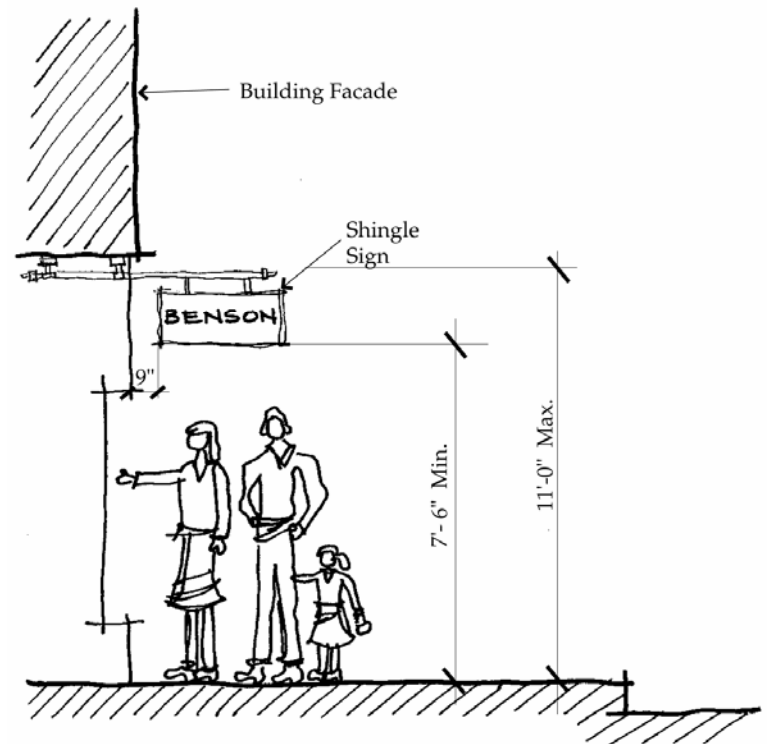
11.2.3 Non-Residential Uses

Each business may be permitted to have no more than two signs. Signs may be of the following types:

- a) Awning valence signs: One sign per street level business façade, not exceeding 50% of the awning valence area and centered on the awning valence, is permitted. No additional lighting for the awning valence is permitted.
- b) Wall signs: One sign per street level business affixed to the building wall directly abutting the use and/or occupancy being identified and directly facing a street. A maximum of one square foot of sign per foot of street frontage for the use or occupancy, not to exceed one hundred square feet per sign. Sign colors should be compatible with building colors. External illumination of wall signs is permitted.

- c) Shingle and suspended signs: One sign per street level business. A maximum of four square feet of sign area. Shingle and suspended signs should be of wood or material simulating wood, suspended from the underside of a pedestrian canopy or awning directly adjacent to the business identified on the sign. Shingle and suspended signs shall be oriented perpendicular to the adjacent wall of the building being identified and attached in a manner acceptable to the Building Official that prevents swinging. No portion of the sign shall be more than 11 feet from the finished grade of the ground below the sign nor less than 7-1/2 feet above any pedestrian walkway. The provisions of this section supersede those of Section 16.32.070 of the City Code.

GUIDELINES FOR SIGNAGE



- d) Street Level Window Signs: Two signs per street level business per building façade located on or adjacent to the inner window surface and directly facing a street. Signs may not exceed 15% of the window area or nine square feet, whichever is smaller. The use of gold or silver leaf or metallic vinyl is encouraged, with a drop shadow behind the letters to increase visibility.

- e) Upper Floor Window Signs: A maximum of one window sign per upper floor business per building façade, with each window sign not to exceed 15 square feet each and not directly illuminated. The use of gold or silver leaf or metallic vinyl is encouraged, with a drop shadow behind the letters to increase visibility. Colors should match or complement the street level display window sign of the same building.

11.3 PROHIBITED SIGNS

The following types of signs as defined in Chapter 16.20 of the City Code are prohibited within the Downtown Specific Plan area:

- a) Free-standing signs, including, but not limited to, pole-mounted signs.
- b) Off-site signs, including, but not limited to, billboards.
- c) Electric signs.
- d) Sign towers.
- e) Roof signs.

Chapter 12 – Parking

This Chapter presents recommendation for Downtown parking strategies to meet future demand and establishes standards for bicycle parking in the Downtown Specific Plan area. The Chapter is organized as follows:

- 12 Parking
 - 12.1 Parking Policies and Supply
 - 12.2 Standards for Off-Street Parking and Loading
 - 12.3 Parking Strategies
 - 12.4 Bicycle Parking Standards

See Appendix B, Existing Conditions, for additional information on current parking policies, supply and utilization.

12.1 PARKING POLICIES AND SUPPLY

12.1.1 Parking Policies

Because of the small size of the Downtown, its pedestrian character, and the typically pleasant weather, a “park once” policy is recommended for the Downtown. Rather than driving from one Downtown use to another, visitors should be encouraged to park once and walk to one or more destinations within the Downtown. Residents and employees should similarly be encouraged to walk from their homes or workplaces to Downtown destinations.

Most of the Downtown Core and a portion of the Civic District fall within a parking district established in the 1950s. Except for residential uses, uses in the Parking District are not required to provide off-street parking nor to pay an in-lieu fee (City Code section 22.36.020 D.) Instead, these uses rely on City parking lots and on-street parking.

12.1.2 Future Supply Needs

Several current and planned uses are major generators of Downtown parking demand. In addition to the courts and other County functions, the Intermodal Station and the planned Willows Theater require safe, convenient parking. Downtown special events create additional demand.

To preserve the availability of on-street parking for commercial and civic uses, any residential development that occurs in the Specific Plan area is required to provide dedicated off-street parking in accordance with the standards in Chapter 10 of this Plan. The development concepts prepared as part of this plan assumed that residential parking would be provided on-site and a limited amount of convenience parking could be provided on-site for some new commercial uses. In addition, a central parking structure would provide additional off-street parking supply to support Downtown revitalization.

12.1.3 Future Supply Locations

Several options are possible for a Downtown parking structure. Given the small size of Downtown blocks, a full-block site is preferable in order to provide a meaningful number of parking spaces in a structure that is in scale with other Downtown buildings. A structure should be sited where it can serve as many different Downtown uses as possible.

Recommended sites for a central parking structure are shown in Figure 12-1. These include 1) the Kalsbeek property at Marina Vista and Estudillo Streets (opportunity site 9) plus the adjacent Intermodal lot to the north; 2) the block bounded by Alhambra Creek, Marina Vista, Estudillo and Escobar (opportunity site 11); 3) the existing City-owned lot at Ward and Las Juntas (opportunity site 24, plus would likely require acquisition of adjacent parcels); and the existing city-owned lot at Ferry and Marina Vista (opportunity site 13). Of these four options, Site 9 would best serve the Intermodal Station, and Sites 11,13 and 24 would best serve County functions. Another option for increasing the supply of County parking would be to add a single deck to the existing surface lots to the east of the County offices and correctional facility. The existing street grades on the periphery of this site would allow such a deck to have minimal visual impact on the residences on the east side of Willow Street and could allow for vehicular circulation with minimal ramping.

This Plan recommends that parking for the Intermodal Station be located either north or south of the railroad tracks, and that parking for other Downtown uses be located south of the railroad tracks wherever possible.

A Downtown parking structure should have retail space at the base along at least one street frontage and could incorporate office or cultural space as well, as in the example at right from Boulder, Colorado. It should be designed with vertically-proportioned openings and traditional materials and detailing in order to blend in with existing Downtown buildings.



12.2 STANDARDS FOR OFF-STREET PARKING AND LOADING

Refer to Chapter 22.36 of the City Zoning Code for additional standards for off-street parking and loading.

12.2.1 Off-Street Parking Requirements

Residential uses shall provide off-street parking in accordance with the standards contained in Chapter 22.36 of the Zoning Code. Nonresidential uses within Martinez Parking District No. 1, which includes nearly all of the Downtown Core District, are not required to provide off-street parking. Outside the parking district, nonresidential uses in the Specific Plan area shall provide off-street parking in accordance with the standards contained in Chapter 22.36 of the Zoning Code.

12.3 PARKING STRATEGIES

A comprehensive Downtown parking management strategy would assist the City, County, Courts, and Community College District in meeting future parking needs and achieving the goals of the Specific Plan. In addition to relevant recommendations from previous studies, a comprehensive parking strategy should do the following:

- Develop estimates of long-term demand for additional parking for major Downtown uses, with identification of seasonal, daily and hourly variation in demand.
- Develop shared parking strategies to maximize joint use of structures and ensure high parking occupancy throughout weekdays, evenings and weekends.
- Identify preferred location(s) for parking structures.
- Improve “wayfinding” signage to direct auto traffic along Multi-Modal streets into and out of any parking structure.
- Provide convenient and attractive pedestrian connections to and from the parking structure lobby.
- Periodically review the parking requirements of the Specific Plan. As noted earlier, the “park once” concept results in lower overall demand than providing dedicated parking for each use, so it is important to review parking requirements in light of actual utilization.
- Explore opportunities for additional sources of funding under the existing parking district as well as other funding opportunities.
- Encourage walking, bicycling, and greater use of transit, as well as ridesharing, telecommuting and flexible work schedules, to reduce overall parking demand.
- In any parking structure, allow space for car-sharing vehicles should a car-sharing club desire to set up operation in Martinez.

12.4 BICYCLE PARKING STANDARDS

All uses subject to Design Review should provide bicycle parking in accordance with the following standards:

12.4.1 Parking Structures

Public and commercial parking structures should provided a minimum of 20 bicycle spaces. These should be located within view of the entry wherever possible in order to enhance their security.

12.4.2 Residential Uses

For residential development requiring Design Review, one sheltered, secure bicycle parking space per dwelling unit should be required. Bicycle parking may be located in garages, basements, storage sheds, utility rooms, or similar areas that can be secured from unauthorized access and are sheltered from sun and rain. Additional convenience bicycle parking may be provided with exterior racks but does not count toward the sheltered bicycle parking requirement.

12.4.3 Nonresidential Uses

New parking areas created to serve nonresidential uses should provide one bicycle parking space for every 20 vehicle parking spaces, with a minimum of four bicycle spaces.

12.4.4 Downtown Core and Civic Areas

For projects requiring Design Review, bicycle parking for employees and customers should be provided along the street at a rate of at least one space per use. Existing uses are encouraged to provide bicycle parking as well. Spaces should be located on sidewalks or in specially constructed areas such as curb extensions. Individual uses may provide their own parking or spaces may be clustered to serve up to six bicycles. Loop or ribbon racks are recommended and bicycle parking areas may or may not be sheltered. Bike parking should not interfere with pedestrian passage, leaving a clear space of at least 4 feet between bicycles and existing and potential obstructions.

12.4.5 Visibility and Security

Bicycle parking should be visible to cyclists from the street and visible from at least one building entrance and the sidewalk, in order to provide increased security. Bicycle parking areas should be at least as well lit as vehicle parking areas.

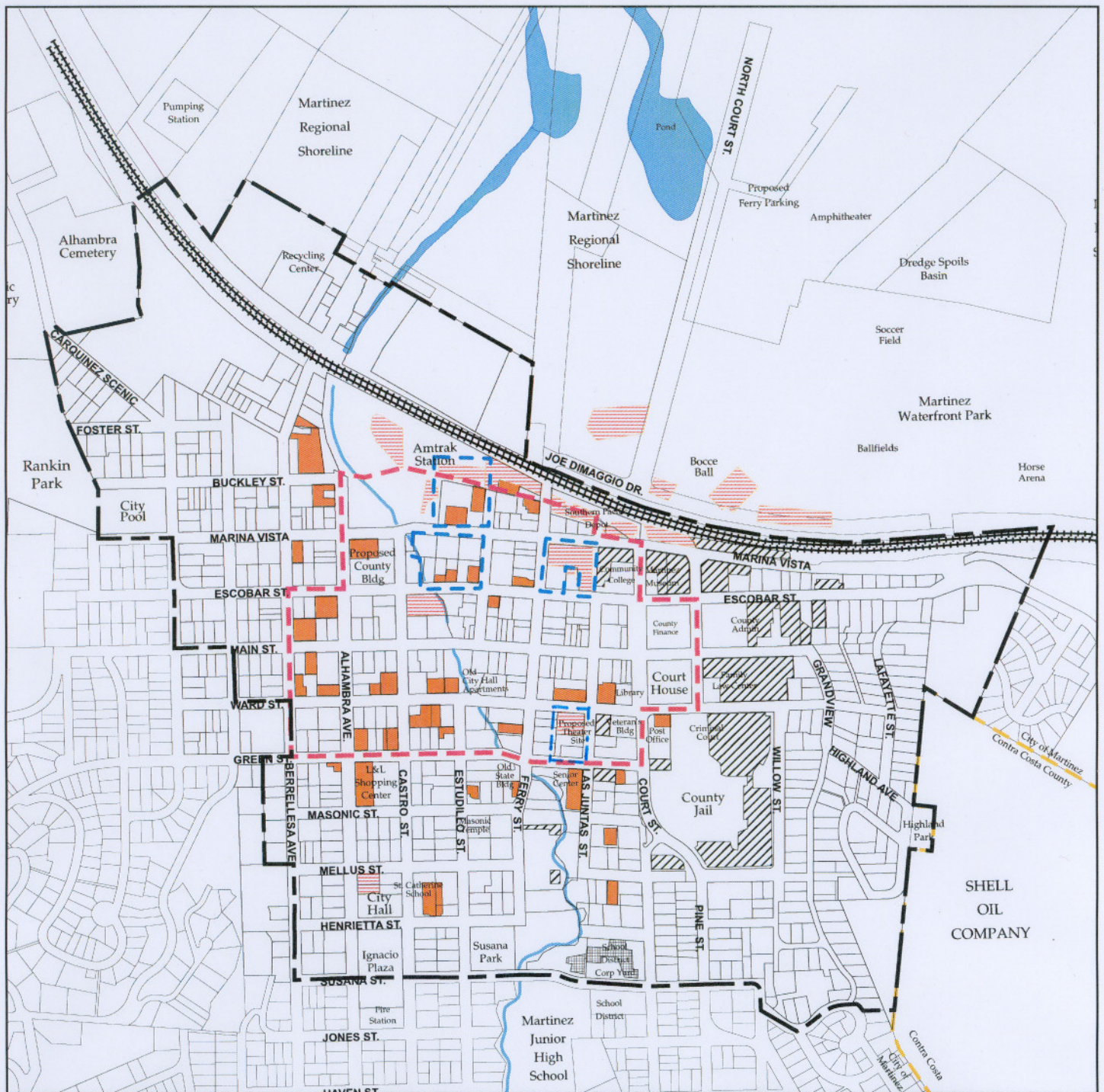
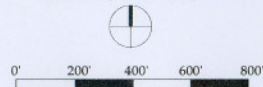


Fig.12-1 OFF - STREET PARKING

Downtown Martinez Specific Plan

Martinez, California



- LEGEND**
- Study Area Boundary
 - ++++ Rail Road
 - Property Lines
 - Alhambra Creek
 - Potential Parking Structure Sites
 - Parking District Boundary
 - Ownership of Parking Lots**
 - City of Martinez
 - Contra Costa County
 - Commercial or Other
 - Martinez Unified School District/
Martinez Junior High School

Sources:
Off-Street Parking: Digitized from Aerial Photography and Site Visit (May 2003)
Downtown Martinez Parking Study

Galtorpe Associates
URBAN DESIGNERS
PLANNERS
ARCHITECTS
Berkeley, California

City of Martinez, California

Chapter 13 – Circulation

This chapter presents circulation concepts for autos, bicycles, pedestrians and transit in the Downtown. This chapter is organized as follows:

- 13 Circulation
 - 13.1 Introduction
 - 13.2 Context
 - 13.3 Traffic Circulation Network
 - 13.4 Pedestrians
 - 13.5 Bicycles
 - 13.6 Transit

13.1 INTRODUCTION

This section presents overall concepts for vehicular and non-vehicular circulation for the Downtown. The plans for auto, transit, bicycle and pedestrian circulation are based on the circulation goals and policies set forth in Chapter 2, Goals and Policies.

The overall circulation plan is based on providing convenient access *to* Downtown for all transportation modes and encouraging circulation *within* Downtown on foot wherever possible. This multi-modal approach recognizes the importance not only of auto circulation, but also of good transit, bicycle, and pedestrian circulation to a successful Downtown environment. The plan therefore focuses on achieving a balanced use of roadway space in the Downtown, so that people can drive into Downtown and find convenient parking, take transit into Downtown, bicycle into and around Downtown, and walk around Downtown, all in a comfortable and safe environment.

13.2 CONTEXT

The current Downtown street system is primarily a grid of two-way streets, with the exception of the Alhambra/Berrellesa and Marina Vista/Escobar one-way couplets and a few additional one-way segments of four blocks or less in length. The principal through streets are Alhambra/Berrellesa, Marina Vista/Escobar, and Court/Pine/Pacheco, due largely to the fact that these are the only routes to and from Downtown with straightforward connections to the regional highway system. These three Gateway Corridors are designated as arterials in the Transportation Element of the Martinez General Plan. The primary circulation system is complemented by a comprehensive grid system of local streets.

There are no signalized intersections in the study area. Traffic volumes are low enough that even in peak hours, all intersections in the Downtown study area function at a high Level of Service (LOS A or B) with stop sign controls.

Grid systems work particularly well in Downtowns due to their inherent simplicity, ease of understanding, and ability to directly serve the diverse land uses typically found in

Downtowns. However, the fact that a number of the Downtown streets are discontinuous where interrupted by Alhambra Creek or County facilities, as well as the several short one-way segments created to provide diagonal parking, tend to make the system somewhat confusing to visitors.

Nevertheless, the street system generally functions very well for the current Downtown land uses. Downtown intersections provide a good level of service and there are few if any traffic circulation problems with respect to either traffic volumes or traffic congestion.

Enhancements to the pedestrian environment could help encourage the desired “park once and walk” environment. Traffic calming measures may be appropriate in certain areas, including pedestrian crossings of the Gateway Corridors and some new local streets.

13.3 TRAFFIC CIRCULATION NETWORK

In order to provide for the efficient access of vehicular traffic within the Downtown, as well as efficient circulation within Downtown, the traffic circulation network identified in Figure 13-1 is proposed. This identifies Multi-Modal/Gateway Corridors, Local Streets, and Pedestrian-Priority Streets. This classification is supplemental to and does not replace the roadway categories described in the City’s General Plan Transportation Element, which remain as specified in that document. The Specific Plan categorization is a functional one, which is intended to describe how various streets are expected to operate with respect to traffic and pedestrian flow in the Downtown.

13.3.1 Multi-Modal Streets and Gateway Corridors

Multi-Modal Streets are those expected to be the principal streets making up the Gateway Corridors that auto and truck traffic, transit vehicles, and bicycles will use to access and travel through Downtown. Their design must balance the needs of all modes of travel, including a section of the Bay Trail and Bay Ridge Trail on Marina Vista. These streets will thus carry the heaviest traffic volumes and will generally have the widest pavement widths, but will also need pedestrian and bike friendly improvements. The Gateway Corridors and Multi-Modal Streets identified in the Plan are:

Alhambra Gateway Corridor

- Alhambra Avenue
- Berrellesa Street

Marina Vista Gateway Corridor

- Marina Vista Avenue (east of Berrellesa)
- Escobar Street (east of Berrellesa)

Pacheco Gateway Corridor

- Court Street (north of Mellus Street)
- Pine Street (between Mellus and Jones Streets)
- Pacheco (between Jones Street and Shell Avenue)

13.3.2 Pedestrian-Priority Streets

Pedestrian-Priority Streets are the primary Downtown retail streets. The pedestrian scale and character of these streets are important elements of the historic small-town feel of the Downtown. On these streets, pedestrian comfort and safety should take precedence over all other considerations. These streets will generally have the widest sidewalk widths, with few if any curb cuts or other interruptions of the pedestrian realm. On-street parking is encouraged to provide a buffer between the sidewalk and moving cars. Traffic volumes will be moderate, and traffic speeds will be deliberately kept low with narrow pavement widths, pedestrian-scale lighting, street trees, and well-marked crosswalks. Two-way traffic is encouraged on Pedestrian-Priority Streets in order to maximize retail visibility. Pedestrian-Priority Streets should have the highest priority for streetscape improvements such as lighting, street trees, sidewalk and crosswalk improvements. Pedestrian Priority Streets may have transit service. The Pedestrian Priority Streets identified in the Plan are:

- Main Street (east of Berrellesa)
- Ferry Street (north of Green Street)
- Las Juntas (north of Ward)
- Estudillo (north of Main)
- Castro (north of Ward)

13.3.3 Local Streets

Other streets within the Downtown area are important for circulation of local traffic to specific land uses and buildings. These streets, which essentially complete the circulation grid, are not expected to carry heavy traffic volumes. Local streets will provide for both connections within and between neighborhoods and thus knit neighborhoods and Districts together, rather than forming barriers between them. Gated entryways into new developments or neighborhoods will not be allowed. Within the Downtown Core and the Civic District, special attention should be given to streetscaping on Local Streets where they intersect the Pedestrian-Priority Streets. These blocks should have the second-highest priority for streetscape improvements, following the Pedestrian-Priority Streets.

The conversion of Local Streets to one-way traffic in order to provide additional on-street parking, as has been done on Castro, Estudillo and the 900 block of Main Street, should be weighed carefully against the resulting loss of clarity and legibility in the Downtown street system. Figure 13-2 shows the existing one-way street system. (Planned street improvements for the new County Clerk/Recorder building include the conversion of Marina Vista to two-way operation between Alhambra Avenue and Castro Street.) To the extent that additional parking supply in the form of a new parking structure reduces the

pressure for on-street parking and in coordination with streetscape enhancements, Local Streets should be converted to two-way traffic with parallel parking wherever possible. Curb bulb-outs should be used at corners and appropriate mid-block locations to increase the sidewalk area available for outdoor seating.

In the Downtown Core, Ward and Green Streets serve an important role as east-west connectors across Alhambra Creek. Of particular concern is Ward Street east of Court Street, an important connection between the Downtown Core, the Civic District, and the Grandview District. Because of the interruption of the street grid for County facilities, auto traffic from several blocks is concentrated on Ward Street, resulting in higher traffic volumes, speeds and accident rates than most other Downtown streets. Traffic calming measures such as curb bulb-outs could help to reduce speeding in such areas.

13.3.4 Recommended Street Improvements

The following street improvements are recommended. These will generally enhance the current grid, improve local traffic circulation within the Downtown, and help make the street system more understandable to visitors.

General

- Do not allow additional street closures. The current Downtown street grid system is essential to the future success of Downtown and should be maintained and enhanced where possible.
- Consider returning one-way Local Streets to two-way operation. This may require construction of a parking structure to replace the diagonal parking that would be removed.

Marina Vista Gateway Corridor

- Consider converting Marina Vista/Escobar to a full one-way pair west of Court Street. This would be undertaken in coordination with repaving, streetscape and parking projects.
- Consider removing one travel lane on Marina Vista between Court, Ferry and Berrellesa to add a Class II (striped) bikeway.
- Consider adding a stop sign for traffic on Marina Vista at Court Street.
- Add stop signs for traffic on Berrellesa at Escobar Street to enhance pedestrian crossings of the Alhambra/Berrellesa couplet where it intersects regional trail corridors.
- Add enhanced pedestrian crossings of Marina Vista at Castro/Intermodal entrance, and Estudillo, possibly at Alhambra, Berrellesa and Las Juntas.

Pacheco Gateway Corridor

- Add stop signs for traffic on Court Street at the intersection of Court and Green Streets.
- Construct bridge* for pedestrians, bicycles and emergency vehicles to connect Court Street over Marina Vista and the railroad tracks to North Court Street. Consider constructing this bridge* to handle vehicular traffic as well. (* or undercrossing)

Alhambra Gateway Corridor

- Extend bicycle lanes on Alhambra from Escobar north to Buckley, and beyond if Alhambra is extended to the north.
- Extend bicycle lanes on Berrellesa from Escobar north, across the railroad tracks to the Regional Shoreline Grangers Wharf staging area.

See Section 13.5 of this Chapter for recommended pedestrian improvements and Chapter 14 for recommended streetscape improvements.

13.3.5 Guidelines for New Local Streets

Any new streets created to serve development shall be designed as Local Streets. Such streets shall have pavement widths narrow enough to slow traffic, while accommodating demonstrated traffic demand and providing adequate emergency vehicle access. New streets should not be wider than needed to accommodate demonstrated traffic demand and should in no case be wider than existing Downtown streets. Any new streets should have sidewalks and pedestrian-scale street lighting. An example of such a section for a new Local Street is shown in Figure 13-3.

Topographic and open space constraints make it likely that many new streets will essentially serve only to access new development, with little or no through traffic possible. However, a few additional connections are desirable to facilitate emergency vehicle access and prevent traffic backups.

Note: not adopted; reserved for potential future action and/or change

Prior to the City rezoning any of the North Shoreline District, an emergency access plan shall be prepared and approved by the City and the Fire District, and adequate emergency access provided prior to occupancy of any new residential units. Prior to approval by the City, the potential environmental impacts of the emergency access plan shall be reviewed pursuant to CEQA. The plan shall provide for a 3 to 5 minute emergency response (as specified in the February 3, 2005 Fire District letter in the Final EIR) to all areas to be rezoned. Said plan shall provide for secondary access or other means to ensure the stated response times. An appropriate funding mechanism shall be adopted to fairly apportion the costs of any access improvements among all benefiting properties, both private and public.

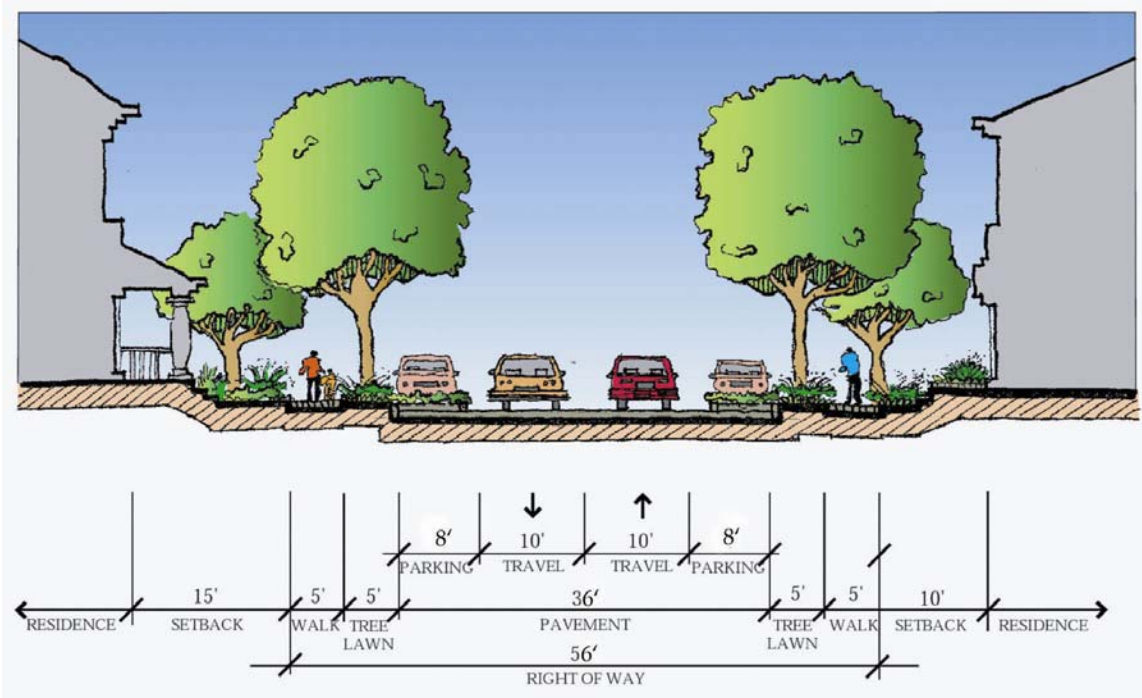


Figure 13-3. Example of a Section for New Local Streets

A bridge over Alhambra Creek and/or a bridge over (or an underpass under) the railroad tracks between Court and North Court would enhance access to the Martinez Regional Shoreline, the City Waterfront Park, and the City Marina property. These bridges could be vehicle bridges or pedestrian bridges built to also carry emergency vehicles. Prior to approval by the City, the potential environmental impacts of these bridges shall be reviewed pursuant to CEQA.

If the Arana property in the Grandview District (Opportunity Site 31) is developed, a new emergency access through the property and along the city-owned former right-of-way should connect Lafayette Street to Lang Street, in order to provide more than one route for emergency vehicle access and permit efficient use of the site. Traffic calming measures would be appropriate on such streets; see Chapter 14 for a discussion of this and other streetscape issues.

13.4 PEDESTRIANS

A comfortable, safe, and enjoyable walking environment is essential to a successful Downtown. While many streets have wide sidewalks and pedestrian circulation is well provided for (e.g. Main Street), there are other areas where pedestrian facilities are less coherent. These include the lack of a good connection across the railroad tracks to the Regional Shoreline entrance at Granger's Wharf, gaps in the planned "creek walk" along Alhambra Creek, and difficult pedestrian crossings at two-way stop controlled intersections along Marina Vista and the Alhambra/Berrellesa couplet. To this end, the Specific Plan includes the following recommendations for pedestrian circulation:

- Stripe crosswalks at all Downtown intersections. Consider using distinctive paving for crosswalks on Pedestrian-Priority Streets, and where such streets cross Gateway Corridor streets.
- Enhance streetscapes with elements such as street furniture, lighting, and brick pavers on routes connecting the Civic District, the Downtown Core, and the Intermodal Station. These include Ferry Street, Main Street, Estudillo Street and Castro Street.
- Require sidewalks and pedestrian-scale street lighting on new streets created as part of development projects.
- Add curb bulb-outs at intersections of streets with on-street parking to reduce pedestrian crossing distances and provide additional opportunities for outdoor seating. This would be done in coordination with sidewalk reconstruction projects.
- Underground the utilities in the few downtown areas where they still exist, to provide more sidewalk space for pedestrians and handicap access.

13.5 BICYCLES

The City's Bikeway Plan, part of the General Plan Transportation Element, notes that "[t]he bicycle, although often regarded as a vehicle for recreational purposes, can be an important means of transportation. Inexpensive, energy-conserving, and non-polluting, the bicycle can serve as an alternative to the automobile and can contribute to alleviating traffic congestion."

The Bikeway Plan calls for continuous striped (Class II) bike lanes on the Gateway Corridor Streets that lead into and out of Downtown: Alhambra/Berrellesa, Marina Vista/Escobar, and Court/Pine/Pacheco. West of Berrellesa, a signed (Class III) bike route along Marina Vista and Talbart would connect to Carquinez Scenic Drive, a route that is also a dedicated segment of the Bay Trail and the Bay Area Ridge Trail. The existing and planned bikeway network is shown in Figure 13-3.

It is the intent of the Specific Plan to facilitate bicycle access to Downtown and links between Downtown and the regional trail systems by implementing appropriate bikeways along these routes. Striped bike lanes on Court Street may not be feasible until a parking structure is in place, since they could only be created by removing parking on one side of the street.

Currently, Class II bike lanes exist in the study area only on Alhambra and Berrellesa (from Marina Vista to Henrietta) and on Escobar and Marina Vista east of Pine Street.

The Specific Plan includes the following recommendations for improvements to bicycle circulation:

- Add a Class II (striped) bike lane on Marina Vista between Pine, Ferry, and Berrellesa Streets.
- Add a Class II (striped) bike lane on Alhambra and Berrellesa Streets between Susana and Henrietta Streets, and from Escobar north to Buckley and beyond, across the railroad tracks to the Grangers Wharf staging area.
- Where bicycle lanes are not provided, sign all Multi-Modal Streets as Class III bikeways.

Please see Chapter 12, Parking Standards, for bicycle parking standards.

13.6 TRANSIT

Transit will play an increasingly important role in the future development of Downtown. As development occurs, transit is ideally suited to moving additional people in and out of Downtown Martinez while minimizing impacts on the roadway system. The intent of the Specific Plan is to ensure that transit continues to perform as a viable alternative to the automobile, particularly for commute trips into and out of Downtown.

13.6.1 Train Service

The Martinez Intermodal Station is located on Marina Vista near the northwestern boundary of the project study area. Four Amtrak routes serve this station. These routes include:

- Capitol Corridor (service from San Jose to Sacramento, 24 trains daily)
- San Joaquins (service from San Francisco to Bakersfield, 10 trains)
- California Zephyr (service from Chicago to Emeryville, 2 trains)
- Coast Starlight (service from Seattle to Washington, 2 trains)

The Capital Corridor operates 24 trains per day with one to two hour headways during the day. The San Joaquin service is ten trains per day and the California Zephyr and Coast Starlight provide twice daily service. There are a total of 38 trains per day that stop at the Amtrak station.

The Capitol Corridor plans to expand to 32 trains per day by 2011. In addition, there is a plan to add commuter service to the inter-city train service of the Capitol Corridor.

The City should continue with its existing plan of providing additional parking for the Intermodal Station, either north of the tracks or south of the tracks. In addition, the City should improve access to the Intermodal Station by upgrading Marina Vista by providing a bike lane, enhanced pedestrian crossings and new streetscape. These improvements would support the change in land use proposed by this plan along Marina Vista from service commercial to the mixed residential/commercial land use of the Downtown Core.

The City should support the addition of commuter train service on the Capitol Corridor.

13.6.2 Bus Service

The Central Contra Costa Transit Authority or CCCTA (known as the County Connection) operates most of the bus service in the Downtown Martinez area, with five routes serving Downtown and a major transfer point at the Martinez Intermodal Station. County Connection routes connect Downtown Martinez with nearby BART stations and other destinations throughout Martinez, Pleasant Hill and Concord. These routes generally operate on headways ranging from 15 minutes to an hour. In addition to the Intermodal Station, bus stops are provided near the County Courthouse and at the western edge of the Main Street retail corridor.

In addition, weekday-only transit connections to Downtown Martinez from East and West Contra Costa County are provided by Tri-Delta Transit (one route with 60-90 minute headways) and WestCAT (one route with 30-60 minute headways).

The City should continue to work with the transit district to provide covered bus shelters in the downtown area.

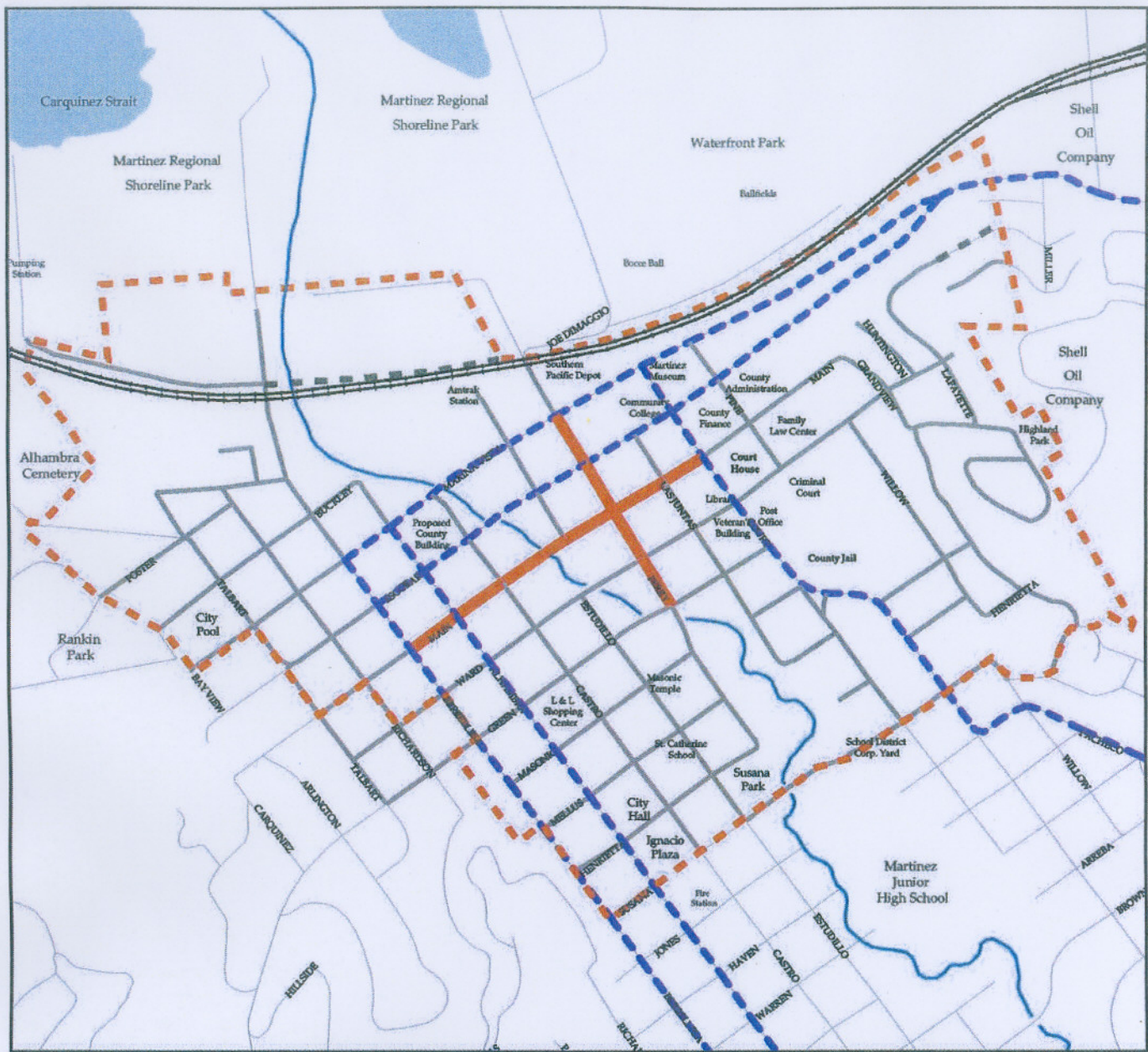
13.6.3 Proposed Ferry Service

The San Francisco Bay Area Water Transit Authority (WTA) identified the City of Martinez as a potential location for a new ferry station as part of a ferry line from Antioch to the San Francisco Ferry Building. Approximately 30 round-trip ferry trips would be provided per day with 15-round trips on the weekend. The total trip time would be 80 minutes (Antioch to San Francisco) with the trip from Martinez to San Francisco requiring approximately 53 minutes. Initial ridership estimates for this line are 1,600 total riders per day with a total of 600 daily riders using the terminal in Martinez.

According to the WTA, construction of a Martinez ferry station, if funded, would occur in the year 2009 to 2011 timeframe, with service to begin in year 2012. The station site would be the old ferry slip near the public fishing pier. If ferry service were operated from the pier location, shuttle bus service to the Intermodal Station would be provided by extending CCCTA routes from the Intermodal Station. This shuttle service would also

facilitate transfers to other transit services (bus and commuter rail) at the Intermodal Station. The City recently completed a Ferry Plaza at the marina, which is designed with bus bays for the future ferry service.

The City should continue to support projected ferry service from Martinez to San Francisco, and continue to pursue construction of ferry facilities at the Martinez Marina.



LEGEND








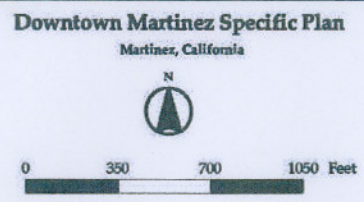
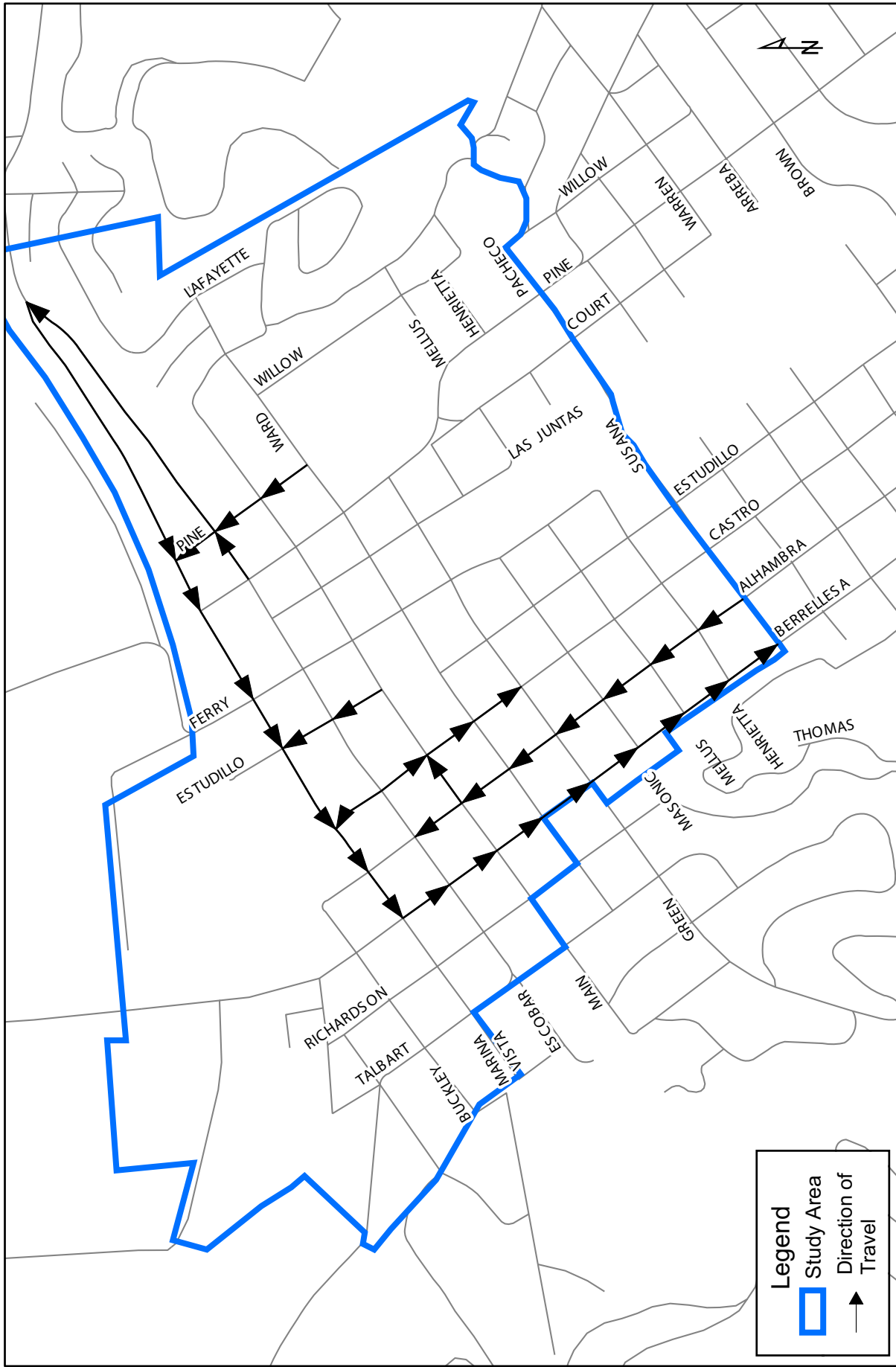
-  Study Area
-  Alhambra Creek
-  Rail Road
-  Multi - Modal Street/Gateway Corridor
-  Pedestrian Priority Street
-  Local Street
-  Proposed Local Street Connection

Fig. 13-1 TRAFFIC CIRCULATION NETWORK




 Galthorpe Associates
 URBAN DESIGNERS
 PLANNERS
 ARCHITECTS
 Berkeley, California
 City of Martinez, California



One Way Streets
 Figure 13-2



Chapter 14 – Streetscape Improvements

This chapter presents streetscape design concepts for key streets and gateways in the Downtown Specific Plan area. This chapter is organized as follows:

- 14 Streetscape Improvements
 - 14.1 Streetscape Treatments
 - 14.2 Public Signage for Wayfinding and District Identity
 - 14.3 Gateway Corridor Improvement Projects

14.1 STREETScape TREATMENTS

The Downtown area should be landscaped with a palette of street trees and groundscaping keyed to each of the differing street types, to help establish the hierarchy of streets and provide a cohesive theme for the area.

14.1.1 General Streetscape Treatment Standards

a) Size

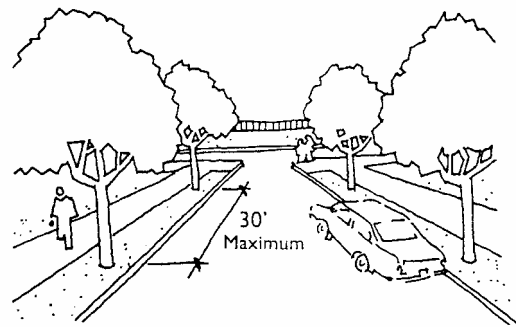
The largest trees should be located on the Multi-Modal and Pedestrian-Priority Streets.

b) Spacing

Street trees should be planted on both sides of streets and spaced no more than 30 feet apart.

c) Species

Each type of street should have one species of street tree for the sidewalk planting area, with an additional street tree type for any in-street parking space trees.



Street trees shall be spaced no more than 30 feet apart on center.

d) Plant Materials

In the Downtown Core, sidewalks are likely to be hardscape, with generous tree grates around street trees. In Residential Districts, a common palette of groundscaping such as turf and low plants is recommended for the tree-lawn area between paving and roadway. A judicious selection of plants includes consideration of site-specific conditions such as shade, wind, moisture, and soils.

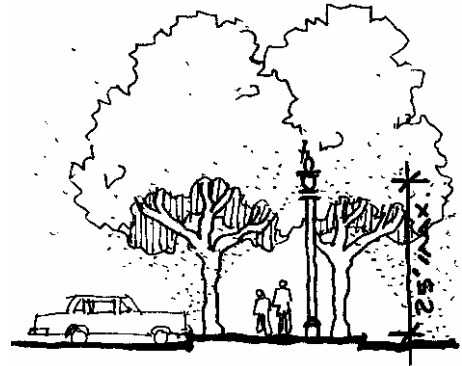
e) Street Lights

Streets must be lit with light standards (fixtures and poles) with a consistent and high quality appearance throughout the area. All lighting should be down-directed to minimize glare and preserve the view of the night sky. Light bollards are also encouraged in areas with high pedestrian activity levels. The height of light standards should correspond to the activities they illuminate:

1. **Along Pedestrian-Priority Streets.** Light standards in environments where pedestrians are the primary focus (e.g. Pedestrian-Priority Streets, most Local Streets and any off-street pedestrian walkways) should be lower in height to create an environment that is more human in scale. These poles should be not more than 25 feet high and spaced approximately 150 to 145 feet apart.

2. **Along Multi-Modal Streets.** Light standards along Multi-Modal Streets must relate to both vehicles and pedestrians at the edge of street, and poles should not be more than 40 feet high; 20 feet in the Downtown Core.

3. **Placement.** To the extent feasible, pairs of light standards should be placed symmetrically along opposite sides of the street.



Lighting along pedestrian paths shall be focused on the pathway and not exceed 25 feet in height

f) **Undergrounding of Utilities**

The City should continue to underground utilities within the Downtown Core. Many areas are done, but there are areas along Marina Vista, Ward and Green Streets that remain. It is important to underground utilities for the following reasons:

1. **Providing space for pedestrians.** The poles and guy wires for overhead utilities take up precious space on the City's sidewalks, which can be better used for pedestrians, and for handicap access.
2. **Providing pedestrian scale lights.** As noted above, standard streetlights are not pedestrian-friendly.
3. **Removing clutter.** Overhead utilities physically clutter the sidewalks, as noted above, and add visual clutter above.
4. **Sense of Place.** Decorative pedestrian-scale streetlights help to establish a sense of place. In the Downtown Core, lights should match those already in place on Main, Escobar and Ferry Streets. A different theme would be appropriate in the Downtown Shoreline residential area.

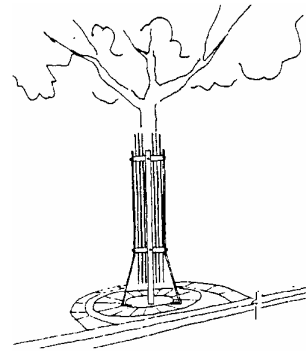
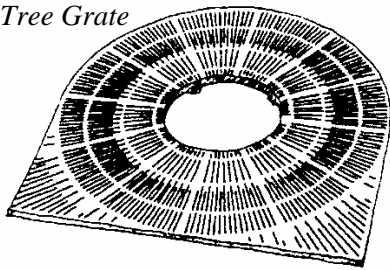
14.1.2 Street Paving & Furnishings

Paving, plants and site furnishings reinforce the character of the Downtown. These features should be consistent with the following recommendations, whether in streets, in parks or plazas, or as on-site landscaping.

1. **Paving.** Brick pavers should be used for all sidewalks in the Downtown Core. Special pavers are recommended in parks, plazas, and in crosswalks on Pedestrian-Priority Streets. Pavers should be durable and of brick, stone, or other

- materials appropriate to the traditional style of the Downtown. Interlocking pavers or stamped, colored concrete should be used to avoid uneven edges. Paver patterns should be simple to ensure quality and consistency.
2. **Tree Grates.** Tree grates should occur along sidewalks and in plazas where a continuous walking surface is needed. Tree openings should be expandable.
 3. **Tree Guards.** Tree guards should extend vertically from tree gates to protect trees in highly active areas. To relate to other site furnishings, tree guard bars should be narrow and vertical, and should be attached to the tree grate. Welds should not be visible. Tree guards should be about four feet in height with openings varying in diameter according to tree species.

Tree Grate



Tree Guard

14.1.3 Pedestrian-Priority Street Treatments

a) Kiosks

Kiosks serve as information booths and/or shelter for small vendors. Kiosk design should be consistent with the architectural style of surrounding buildings. Kiosks should not exceed 25 square feet and must be sited to maintain a clear pedestrian path at least 5 feet in width.

b) Newspaper Racks

Newspaper racks should occur around major pedestrian gathering areas. The design should consolidate all vending boxes into one rack. Rack construction should use masonry elements or metal that compliments other site furnishings in the area, or the architecture of adjacent buildings. The rack should be attractive on all sides and properly anchored.

c) Bicycle Racks

Bicycle racks should be selected that are durable and visually subdued. Based on their performance, “loop”, “ribbon” or “inverted-U” type racks are recommended, and should be sized according to the bicycle parking requirements in Chapter 12. A durable powder-coat painted finish is recommended, in a color consistent with other Downtown street furniture.

d) Trash Receptacles

Throughout the Downtown, trash receptacles should be provided. Receptacles should have vertical metal bars and be painted a consistent color such as a deep forest green to match other recommended features. To avoid overflow, receptacles should be sized to be at least a 30 gallon capacity, especially in commercial areas, and should be properly

anchored. Multiple coats of a powder-coating or comparable finish are recommended for durability.

e) Planters

Planters should be simple in form. Round and square types are recommended. Material should consist of cast stone or precast concrete. Planters should be at least three feet in diameter. Where planters are called for, group various sizes in clusters to enrich streetscapes and plazas.

14.1.3 Traffic Calming Features

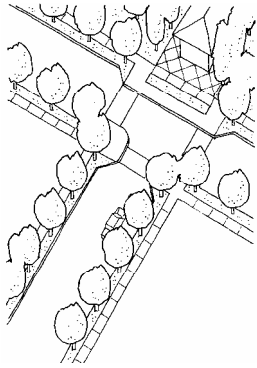
The design of an interconnected street network within residential neighborhoods should include provisions which discourage fast, through traffic on Pedestrian-Priority and Local Streets. Traffic measures that restrict traffic at the expense of the overall interconnectedness and coherence of the area should be avoided.

“Bulb-outs,” textured crosswalks and raised intersections are traffic calming measures that may be used even on Gateway Corridors and multimodal streets where it is important to establish pedestrian connections.

While consideration must always be given to specific demands and conditions (e.g. traffic volume, proximity to Multi-Modal Streets, turning movements, special vehicle needs, and intersection spacing), recommended traffic management measures include:

Appropriate Street Widths

Each street’s design should be based on its anticipated role within the project and surrounding neighborhoods. Street widths should be narrow enough to slow traffic, while accommodating demonstrated traffic demand and providing adequate emergency vehicle access. New streets should not be wider than needed to accommodate demonstrated traffic demand and should in no case should be wider than existing Downtown streets.



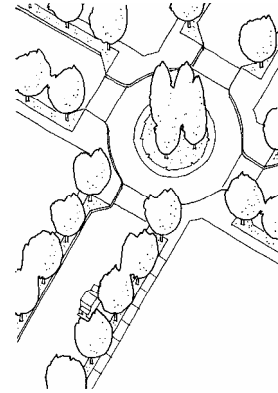
“T” Intersections.
“T” intersections can create dramatic vistas toward the Shoreline.

“T” Intersections

Road alignments may be offset at least 150 feet, forcing turning movements. If carefully sited, “T” intersections can also create prominent vistas to parks and civic features.

Traffic Circles

To slow and divert traffic, small traffic circles may be placed at the center of intersections. These islands may be landscaped, but foliage should be pruned to avoid obstructing drivers’ views. Fountains or sculptures may also be used. Traffic circles function most efficiently when they are used without stop signs.



Traffic Circles. *Small circular islands can be placed at intersections to slow traffic.*

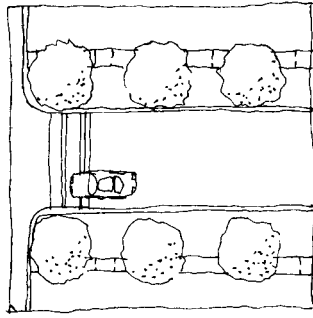
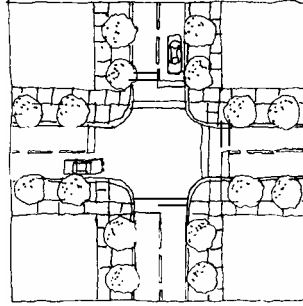
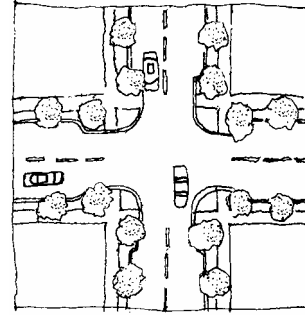
“Bulb-outs,” Textured Crosswalks and Raised Intersections

These features can be used singly or in combination. Bulb-outs slow traffic and reduce pedestrian crossing distances by narrowing the curb-to-curb dimension of the street, either at an intersection or mid-block. They are typically created by eliminating the parking area next to the intersection in cases where turn lanes are not needed. Crosswalks can be textured by means of special pavers or other treatment, alerting drivers that the area being traversed has a special identity for pedestrians. Raised intersections raise the entire intersection a few inches to make drivers aware of pedestrian crossings.

Inappropriate Measures

Traffic calming measures that are not recommended for the Specific Plan area include the following:

1. **Speed Bumps.** Speed bumps traverse travel lanes with raised strips that are typically 3-4 inches high. The need for speed bumps is symptomatic of road designs that fail to slow traffic. While speed bumps may offer an appropriate method for slowing traffic within certain pre-existing conditions, other methods for slowing traffic should be employed on new streets.
2. **Street Closure and Forced Turn.** Using barriers and diverters works against the creation of an interconnected street network and is not recommended.

*Textured Crosswalk**Raised/Texture Intersection**Bulb-Outs*

Traffic Calming Measures. A combination of techniques may be used in areas of heavy pedestrian or bicycle traffic to effectively “tame” traffic.

14.2 PUBLIC SIGNAGE FOR WAYFINDING AND DISTRICT IDENTITY

Downtown Martinez currently has several varieties of public signage dating from different eras and in various states of repair. Recent banner signs on streetlamps coexist with wooden street signs from the early 1980s. The City should consider developing a comprehensive wayfinding strategy, given the new uses contemplated in the Downtown, the circulation concept discussed in Chapter 13 of this Plan, and the regional and local trail segments located Downtown. Such a strategy should develop a coordinated design palette for several different types of signage:

- a) Signage to direct auto traffic from Multi-Modal Streets to parking opportunities and to the Intermodal Station.
- b) Signage for bicycle routes.
- c) Signage for regional trails (the San Francisco Bay Trail and Bay Area Ridge Trail/Juan Bautista de Anza Trail)
- d) Signage for the proposed “creek walk” along Alhambra Creek
- e) Street map kiosks to help orient visitors.
- f) Transit signage, including transit stop locations and information.
- g) Historical markers and plaques.



Map kiosks can help pedestrians locate nearby destinations and encourage walking within the Downtown.

Many routes, such as Escobar Street, serve multiple functions, making coherent signage all the more critical. On these routes, signs for bicycle routes and trails should be grouped onto a single pole for a more coherent streetscape.

14.3 GATEWAY CORRIDOR IMPROVEMENT PROJECTS

The City should improve Gateway Corridors as they reach the downtown so that they have consistent streetscaping of brick sidewalks, street trees and decorative street lights to match those existing in Downtown, plus bicycle lanes. These improvements will help identify the mixed use downtown area, and give a sense of place and arrival. Where there are overhead utilities, they should be undergrounded to provide more sidewalk space for pedestrians and handicap access, as well as reducing visual clutter.

Gateway Corridors to Downtown should also be identified by gateway monuments and entry features. There are existing gateway monuments at Alhambra and Bertola Streets, where Alhambra Avenue splits into the Alhambra/Berrellesa couplet, and at Marina Vista Avenue where it merges with Escobar Street. These “gore points” where two streets converge into one are natural locations for gateway monuments. An additional opportunity for a Downtown Gateway entry feature would be at Pine and Mellus Streets, where Pacheco Boulevard transitions into Court Street, since many visitors to the County complex arrive this way.

The Intermodal Station is, of course, another important gateway to Downtown Martinez. Signage should direct pedestrians exiting the station east to Ferry Street, where the old train station, the entrance to the waterfront parks, and the historic buildings lining Ferry Street create a strong mental image of Downtown Martinez as the intersection of station, waterfront and historic Downtown. The historic train display and September 11 memorial in this area already contribute to the civic focus; additional plantings, coordinated signage or special paving could help reinforce the sense of arrival at this location. A gateway treatment here at the heart of Downtown should incorporate a map kiosk to help visitors plan their expeditions to the many Downtown attractions.

Marina Vista Corridor

Following is a description of the project elements needed in the Downtown portion of this corridor. These improvements are critical to help integrate this area, which this Plan redesignates from service commercial to mixed use residential, into the expanded Downtown Core. In the past, Marina Vista functioned more as an industrial truck access route and barrier that separated this area from the downtown.

1. Add a small overlook park around the Marina Vista entry monument. (It currently consists only of a sign.) Add benches for pedestrians, and perhaps some pull off parking spaces. Removal of the non-native eucalyptus trees would open up the view over downtown Martinez and the waterfront park.
2. Add consistent streetscaping on Marina Vista: brick sidewalks, street trees and decorative streetlights. This new streetscape should begin at the bottom of the hill and extend past the Intermodal entrance to Berrellesa.

3. Extend the bicycle lane from Pine Street west to the Intermodal entrance, and then on to Berrellesa, where it would tie into the existing Berrellesa bicycle lane. This will also act as traffic calming, by eliminating the ability of cars to pass slower traffic. This will also make it easier for pedestrians to cross Marina Vista.
4. Add enhanced pedestrian crossings of Marina Vista to the area around the Intermodal, at Estudillo, Castro and Alhambra/Berrellesa.
5. Underground the overhead utilities that still exist between Ferry and Berrellesa.
6. Provide improvements that relate to the Bay Trail and Ridge Trail, such as the overlook park mentioned above, and ultimately a pedestrian bridge crossing the railroad tracks, connecting Court Street and North Court Street.
7. The Pedestrian Priority streets of Castro and Estudillo that provide pedestrian access to the downtown from the Intermodal Station should be improved with the same consistent streetscape as other downtown streets: brick sidewalks with street trees and decorative streetlights. Overhead utilities should be undergrounded as stated above.

The Marina Vista Gateway corridor begins at I-680, runs through the Shell refinery, and enters Downtown at the Marina Vista-Escobar “Y”. The City rebuilt Marina Vista through the refinery with landscape medians and bicycle lanes in the mid 1990’s; some landscaping should be provided on the north side to screen the railroad tracks.

The portion of this corridor near I-680 will be rebuilt by Caltrans and the City at the completion of the Benicia Bridge project, currently set for 2010. This portion will have medians, bicycle lanes and a pedestrian path. These improvements will connect with the new pedestrian bicycle lane that Caltrans will provide on the Benicia Bridge as part of the Bay Trail. The improvements will also ultimately extend to the east on Waterfront Road to connect to the Iron Horse Trail and Point Edith.

Alhambra Corridor

Following is a description of project elements needed for the downtown portion of this corridor:

1. Add an entry feature near Green and Alhambra.
2. Extend the bike lane on Alhambra from Escobar to Buckley, and beyond if Alhambra is extended along the Creek.
3. Extend the bike lane on Berrellesa from Escobar across the railroad tracks to the Grangers Wharf staging area.
4. Provide consistent streetscaping: brick sidewalks, street trees, decorative sidewalks, from Green to Buckley, and beyond if Alhambra is extended.

The Alhambra Gateway Corridor begins at Highway 4, and runs for several miles before reaching the Downtown. The City beautified this section and added bicycle lanes in the late 1990’s and early 2000’s.

Pacheco Corridor

Following is a description of project elements needed for the downtown portion of this corridor:

1. Add an entry feature at Mellus and Court, or at Pine/Jones/Pacheco.
2. Provide consistent streetscaping: brick sidewalks, street trees, decorative sidewalks, from Mellus to Marina Vista.
3. Extend bicycle lanes from Jones Street to Marina Vista.
4. Construct a pedestrian-bike-emergency vehicle bridge over the railroad and Marina Vista to connect Court and North Court. Possibly construct this bridge as a regular vehicle bridge, or as an undercrossing.

The Pacheco Gateway Corridor begins at I-680, but is in the County for several miles. The City portion begins near Shell Avenue. The City constructed bicycle lanes, undergrounded the utilities and installed street trees for the portion between Potter and Jones in the late 1990's.

Chapter 15 –Open Space and Infrastructure

This Chapter discusses open space and infrastructure for the Specific Plan area. The intent of this Chapter is to specify the open space and infrastructure facilities proposed to be located in the area and needed to support the land uses. This Chapter is organized as follows:

- 15 Open Space and Infrastructure
 - 15.1 Open Space
 - 15.2 Infrastructure

15.1 OPEN SPACE

15.1.1 Open Space and Trail Network

The Downtown Specific Plan Area lies in the narrow valley of Alhambra Creek, adjacent to over 400 acres of open space in the Regional Shoreline and Waterfront Park. Cemeteries and the Carquinez Strait Regional Shoreline form a green backdrop to the west. Hills to the west and east provide pleasant views of trees and even olive orchards, reinforcing the sense of Downtown as an urban peninsula reaching into a spectacular natural setting.

Several local and regional trail corridors intersect in Downtown Martinez, making it a natural place for trail users to embark, rest or have a meal along their way. Within the Martinez Regional Shoreline, nearly three miles of trails provide close-up views of the marshland habitat and distant views of the Carquinez Strait.

The longstanding goal of a “creek walk” along Alhambra Creek has been largely realized through the creation of the creekside plaza between Ward and Main Streets and the landscaped creek channel improvements from Marina Vista north to the railroad.

This Plan proposes the incremental continuation of the creek walk as adjacent properties and streetscapes are improved in accordance with the Alhambra Creek Plan, as well as a new creekside park with a children’s play area near Green and Ferry. This location is important because it is the intersection of the Alhambra Creekway and the termination of the Ferry Street retail corridor.

Two regional trail systems intersect at the Nejedly Staging Area in the hills west of Downtown. The **San Francisco Bay Trail**, a 400-mile trail corridor encircling the bay, of which 240 miles have been developed, runs along the southern edge of the Martinez Regional Shoreline and through Downtown. This Bay Trail segment also forms part of the **Juan Batista de Anza Trail**, a proposed 900-mile multi-use trail system from Mexico to San Francisco commemorating the route of explorer Juan Batista de Anza. The **Bay Area Ridge Trail** is a planned 400-mile multiple-use trail connecting parks and preserved open spaces along the ridgelines surrounding San Francisco Bay, of which 230 miles have been completed. A dedicated segment of the Ridge Trail runs to the west of

the study area through the Franklin Hills, crosses the Bay Trail/De Anza Trail at the Nejedly Staging area and continues into Downtown. A planned extension of the Ridge Trail to Solano County would run east from Downtown along Escobar and Marina Vista, coterminous with the Bay Trail.

Figure 15-1 illustrates the trail systems described above.

In addition to regional and local trails, self-guided historical walking tours are recommended to provide residents and visitors with opportunities to experience the unique combination of historical, cultural and natural elements that are part of Downtown Martinez. These trails should be illustrated by maps geared primarily to pedestrians and bicyclists. These “urban trails” could be varied in length and emphasis. For example, trail maps and self-guided or guided tours could be related to architecture, the natural environment, people and places in Martinez history, or various combinations of these topics. A shorter trail could focus on the historic Main Street/Ferry Street/Court Street spines. Longer trails could include the Old Town/Granger’s Wharf neighborhoods as their focus, while extending north to encompass Downtown’s natural elements including the Regional Shoreline, cemeteries, and the Carquinez Scenic Drive.

15.1.2 Open Space Standards

This section discusses park and open space standards in the Specific Plan area.

As noted in the preceding section, Downtown Martinez enjoys easy access to a wealth of nearby open space, both active and passive. All portions of the Specific Plan area are within one-half mile or less of the hundreds of acres of developed parkland and natural open space in the Martinez Regional Shoreline and Waterfront Park. Several neighborhood parks and a community park are also located Downtown.

Existing parks and open space in the vicinity of the study area are shown in Table 15-1 below.

Table 15-1, Downtown Martinez Parks and Open Space

Park Type	Service Area	Parks in Study Area	Park acres
Community	1-2 miles	Martinez Waterfront Park	150 acres
		Rankin Park	41 acres
Neighborhood	1/4 to 1/2 mile	Plaza Ignacio	1 acre
		Susana Park	1 acre
		Highland Avenue Park	0.25 acre
Regional	several communities	Martinez Regional Shoreline	278 acres
TOTAL			471 acres

An additional 77 acres of parkland is located within the City of Martinez but outside the study area, bringing the citywide total to 548 acres. If the Martinez Regional Shoreline is excluded, the citywide total is 270 acres of parkland.

The City of Martinez standard for park land is 5 acres per 1,000 people. Thus the 548 acres of existing parks citywide, including the Martinez Regional Shoreline, meets the standard for a population of 94,000. Even if the Regional Shoreline is excluded, the 270 remaining acres of parkland meets the standard for a population of 54,000. The actual population of the City of Martinez in 2002 was estimated at 36,707.

The development of the all the opportunity sites would not displace any existing parkland and would accommodate a maximum population increase of approximately 2,500 inhabitants. Development in Downtown Martinez according to this Plan would comply with the City standard for open space.

15.2 INFRASTRUCTURE

The existing water supply, sanitary sewer, fire protection, and storm drainage systems serving the Specific Plan Area are discussed in a working paper prepared by Bellecci & Associates. This working paper is available from the City of Martinez.

Infrastructure capacity assessments prepared for this Plan found that except for minor improvements of the type ordinarily associated with infill development, the existing systems are generally adequate to serve the level of development expected under this Plan.

Specific utilities are discussed in greater detail below.

15.2.1 Water Service

The consultant team worked with the City Engineer and the City's on-call water system consultant to evaluate the ability of the existing water infrastructure to serve the level of development anticipated in this Plan. After reviewing the water system model results, the City Engineer concluded only typical water system improvements associated with infill development would be required (e.g. no extensive water main extensions outside the project boundary would be needed.) The water system modeling process is discussed below in greater detail.

Bellecci & Associates prepared a base map showing the City's water system, parcels identified as likely candidates for development (opportunity sites) and the streets and parcels unaffected in the Specific Plan build out scenario. The consultant team provided a spread sheet (Table C-4) which indicated the type and extent of development in the Specific Plan area anticipated in the most intensive development concept. Gary Dodson, the City's on-call water system consultant, reviewed the material and analyzed the ability of the Downtown Martinez water system to supply 4,000 gpm fire flows. Dodson ran the City's water model with and without the projected development. He chose two critical parcels as test cases and evaluated the ability to provide the expected fire flow requirements that Consolidated Fire would impose. In both cases the model showed that the existing system would provide ample water.

Two locations of interest were the block north of the railroad between Berrellesa and Ferry (Opportunity Site 2), and the block surrounded by Green, Castro, Ward and Estudillo (Opportunity Sites 21 and 22). The first location was chosen because it is isolated from the rest of the system and is fed by only two pipelines with 10-inch and 12-inch diameters. The other location is fed by five 6-inch pipes and one 8-inch pipe. Under Maximum Day Demand conditions, these two locations both maintain a pressure of over 90 psi. When the demand in each area was increased to 4,000 gpm, the residual pressure in each area was maintained at over 70 psi, which is above the minimum residual of 20 psi pressure required during a fire flow event.

For the two specific sites examined, the City Engineer concluded that the only water system improvement that would be needed to support the level of development associated with this Plan would be to loop the water system from Ferry Street north of the rail road tracks through the parcel north of the Intermodal (Opportunity Site 2) to the system existing on Berrellesa. The City Engineer estimated that a 10" PVC water line would be sufficient, but the actual size will need to be determined at the time of development.

Water improvements specific to each of the other opportunity sites were not modeled, since the overall water system capacity had been shown to be adequate and the layout, construction materials and other specifics of development that would affect the necessary water improvements are not known at this time. These issues are ordinarily identified during building permit review.

15.2.2 Wastewater

The Specific Plan area is served by the Central Contra Costa Sanitary District (CCCSD). Wastewater is treated at CCCSD's treatment plant, located northeast of the I-680/SR-4 interchange in unincorporated Martinez, before being discharged to Suisun Bay. The CCCSD estimates that its effluent discharge limit should be sufficient to accommodate wastewater expected to be generated from currently planned growth within the CCCSD's service area for the next 35 years, as well as a worst-case assumption of groundwater infiltration.

Generally, wastewater infrastructure is already in place, and new development within the Specific Plan area would only require new hook-ups to the existing wastewater system. Over the course of time, there may be long-term replacement of sewers. However, these would be related to regular maintenance and would not be related to additional development.

15.2.3 Stormwater Capacity

The Specific Plan area has a developed stormwater system. Additionally, the current storm drains serving the Plan area have been brought up to 10-year storm requirements and the part of Alhambra Creek north of Buckley Street has also been restored to accommodate a 100-year storm.

Many of the parcels identified as opportunity site are currently developed or include impervious surfaces. ~~The marshland buffers required for development in the North Downtown Shoreline District would decrease the amount of impervious surface in the area.~~ (Note: not adopted; reserved for possible future action and/or amendment) Significant amounts of additional stormwater are not likely as a result of implementation of this Plan, as virtually all of the parcels within the Plan Area have high levels of impervious surfacing, as is typical in a developed urban area.

15.2.4 Electricity, Natural Gas and Telecommunications

The Pacific Gas & Electric Company (PG&E) provides electricity and natural gas in the study area. The utility grid that serves most of Downtown was installed before office and retail buildings required the level of energy that they do today. Some tenants (restaurants, bakeries, etc.) could require utility upgrades that exceed the capacity of the existing Downtown utility grid.

PG&E is obligated to upgrade infrastructure and provide services under California Public Utilities Commission Tariff. However, PG&E's method for financing upgrades to its Downtown Martinez system requires owners who undertake significant rehabilitation to pay for their necessary utility upgrades themselves. If the infrastructure has already been upgraded by another property owner, owners of neighboring buildings are only responsible for connecting their buildings to that infrastructure.

Chapter 16, Implementation, discusses assessment districts, one way that the cost of necessary utility grid upgrades could be spread equally among all benefiting property owners or businesses in the area requiring an upgrade.



LEGEND

- Study Area Boundary
- Rail Road
- Property Lines
- Alhambra Creek
- Existing Martinez Regional Shoreline Trails
(Source: East Bay Regional Park District; California Coastal Conservancy)
- Proposed Martinez Regional Shoreline Trails
(Source: East Bay Regional Park District; California Coastal Conservancy)
- Proposed Trail - Alhambra Creek Enhancement Plan
(Source: Alhambra Creek Enhancement Plan, City of Martinez, April 1992)
- Existing/Proposed Bay Area Ridge Trail
(Source: East Bay Regional Park District)
- Planned Bay Trail (Source: East Bay Regional Park District)
- Parks / Open Space
- Proposed Park / Playground
- Pedestrian Priority Streets

Fig.15-1 OPEN SPACE & TRAILS SYSTEM

Downtown Martinez Specific Plan

Martinez, California



0' 200' 400' 600' 800'



City of Martinez, California

Chapter 16 – Implementation

This section provides the framework for implementing the Specific Plan. It includes a variety of specific infrastructure improvements and ongoing programs, known as catalyst projects or catalyst actions that will help Martinez achieve the goals and policies of the Plan. This Chapter is organized as follows:

- 16 Implementation
 - 16.1 Priority Catalyst Projects
 - 16.2 Financing and Funding Mechanisms
 - 16.3 Implementation Matrices

16.1 PRIORITY CATALYST PROJECTS

Several catalyst projects require action and attention to ensure the effectiveness of the strategies outlined in the Specific Plan. These priority catalyst projects are:

- Changing land use regulations;
- Improving the electric and gas utility grid;
- Creating an unreinforced masonry (URM) program; and
- Establishing an historic preservation district.

16.1.1 Changes to Downtown Land Use Regulations

The bulk of this Specific Plan identifies and describes changes to the land use regulations that are effective immediately pending approval of the Plan. Most of these changes can be found in Chapter 10: General Design Standards and Guidelines and Chapters 3-9, which describe and set standards for the various Plan Districts. The new set of land use regulations serves many purposes also identified in these chapters, but in part is intended to catalyze positive economic and physical change in the Downtown Specific Plan area through private development activity.

Upon final approval of this Specific Plan and the subsequent Environmental Impact Report, these land use changes will be the first catalyst project to be implemented by the City. Future financial burdens to the City as a result of changing land use regulations will be in the form of review and approval of private development, but these relatively minor costs will be compensated for by development and permit fees. In addition, this catalyst action will likely enhance the City’s overall property and sales tax revenue through private reinvestment.

16.1.2 Improvement of the Utility Grid

The utility grid that serves most of the Downtown was installed before office and retail buildings required the levels of energy that they do today. Office heating, ventilation, and air conditioning (HVAC) systems require a substantial amount of electricity and gas, and potential new retail tenants with on-site production facilities (restaurants, bakeries, etc.) could require utility hookups that exceed the capacity of the existing Downtown utility grid.

As a result of the increasing demand for energy and the deficient infrastructure currently available in the area, some individual Downtown property owners reportedly have absorbed the full costs of upgrading parts of the utility grid to suit the needs of their tenants.

According to property owners who have upgraded their properties, and to an electrical engineering specialist, Pacific Gas and Electric's (PG&E) method for financing upgrades to its Downtown Martinez system requires owners who undertake significant rehabilitation to pay for their necessary utility upgrades themselves. If the infrastructure has already been upgraded by another property owner, owners of neighboring buildings are only responsible for connecting their buildings to that infrastructure. As an example, one property owner might pay \$100,000 for the installation of a new transformer on the electrical grid during rehabilitation, after which a neighboring owner would only have to pay \$10,000 to connect their building to that transformer.

This financing system makes it extremely difficult to stimulate the process of revitalizing the Downtown commercial core. The few property owners who are willing to invest in the Downtown early in the revitalization process would be burdened with higher costs than those who decide to invest in upgrading their buildings once revitalization is well underway.

To stimulate reinvestment in Downtown structures, the costs of utility upgrading could be spread equally among all benefiting property owners or businesses in the area requiring an upgrade. This could be done if the City identified the exact area requiring such an upgrade as well as the costs of this upgrade, and organized an assessment district to finance improvements to the utility grid in areas where such improvements are needed to stimulate property reinvestment.

16.1.3 Unreinforced Masonry Program (URM)

There are many unreinforced masonry buildings in Downtown Martinez that will require significant retrofitting to accommodate the types of tenants willing to pay higher rents. Likewise, many older, non-masonry buildings in the Downtown have structural issues that could make them unsafe in an earthquake and thus undesirable for new tenants. These buildings would also require a significant investment from property owners for structural upgrading.

Unfortunately, the costs of seismic upgrading are not compensated with proportionally higher rents. Many higher rent tenants will only consider leasing a retrofitted building, but the current Downtown commercial real estate market is not strong enough to enable property owners to cover the cost of retrofitting by increasing their tenants' rental rates sufficiently.

Cities in California maintain one of three types of URM Mitigation Programs. Mandatory Strengthening programs require owners to strengthen their buildings within times prescribed by each local government. Voluntary Strengthening Programs establish seismic retrofit standards and require owners to evaluate the risks in their buildings. Owners write public letters indicating their strengthening plans and schedules. These programs are popular but are only slightly more effective than notification only. Notification Only programs are when local governments write letters to owners stating that their building type has been known to perform badly in earthquakes.

Martinez currently maintains a Notification Only Program, which has resulted in low levels of URM mitigation. A more intensive program would ensure increased earthquake safety in the City, but would likely also require an increased public financial commitment that would essentially subsidize property owners who would otherwise be unable to afford seismic improvements.

Other Bay Area cities have adopted a variety of financial incentive programs to encourage URM retrofitting. Cities such as Morgan Hill, Oakley, and Pittsburg waive or reduce the building permit fees usually required for development when the owner of a URM building applies to upgrade it. Other cities provide grants or low interest loans to property owners, which are financed either through redevelopment agency monies (Emeryville, Vacaville, Fremont), general obligation bonds (San Francisco), or Special Assessment Districts (San Leandro). The parameters of these loan and grant programs vary, but often limit the amount of public financing to around \$50,000 per structure.¹

Case Study Test for a URM Program

Strategic Economics' *Historic Preservation Case Study*, available from the City of Martinez, tested the financial feasibility of retrofitting a brick building in Downtown Martinez without public financial support. A Downtown property owner volunteered his URM building as a case study for four upgrade scenarios: the first two scenarios provided minor tenant improvement upgrades and cleaning but did not require URM mitigation measures. The second two scenarios added URM mitigation costs to the upgrading process.

Each scenario was measured on the basis of current market conditions in the Downtown. The results of the financial analysis showed that the property owner could eventually pay for the improvements in the first two scenarios through an increase in rents, but that higher rents could not justify the additional debt added from URM mitigation costs. In other words, the owner of the case study building would need public financial support to acquire the loans needed to retrofit his building.

16.1.4 Historic Preservation District

While there is clearly symbolic and ambient value in the unique historic buildings that make up the Downtown, translating this into commercial and tourist-generating value is a challenge. The long-term deferred investment in basic maintenance for older commercial buildings has resulted in a variety of structural problems that require expensive improvements before superficial rehabilitation can take place.

Many rehabilitation projects incur unexpected replacement and reinforcement costs and experienced property developers in the Downtown have come to expect the costs associated with engineering improvements of their buildings to be unpredictable. This has made it difficult for owners to attract private financing, because private lenders do not want to absorb the risks associated with unpredictable construction costs.

¹ From the Northern California Chapter of the Earthquake Engineering Research Institute:
http://www.quake06.org/quake06/best_practices/IRVPOP.html

The alternative to rehabilitation in Martinez is deferred maintenance, which ultimately will lead commercial buildings to an uninhabitable state. While rehabilitation is costly and difficult to complete, it is the only option if the community of Martinez wants to preserve its historic buildings for future use.

The Specific Plan implements a Downtown Historic Overlay District, as described in Chapter 6. The District will allow owners of qualified historic properties in the Downtown Core and Civic Districts to use the State Historical Building Code (SHBC) when making improvements to their properties. Because the SHBC is more flexible than the Building Code adopted by the City of Martinez, it will be easier for property owners to maintain and upgrade their buildings in ways that are both architecturally appropriate and cost effective. Additionally, local recognition enables historic properties to apply for property tax relief from the City in exchange for restoration and maintenance via the Mills Act (see State Historic Restoration Incentives, Appendix A).

The creation of a State or Nationally Registered Historic District or the listing of individual buildings on the State or National Register, a much more complicated process than the implementation of the Downtown Historic Overlay District, would make many of the buildings in the Downtown eligible for federal and state funding restoration sources and tax credits. This could significantly reduce the costs of restoring these buildings for use by a variety of retail tenants who would otherwise not consider locating in Downtown Martinez. Please see Appendix A for further explanation.

16.2 FINANCING AND FUNDING MECHANISMS

Table 16-2 identifies a variety of mechanisms that the City can use to finance its Catalyst Projects. This list should be approached as a menu of financing choices rather than as a recommendation for any particular financing strategy. Identifying and pursuing any of the financing options described in Table 16-2 is a policy decision that should be made by the City on a case-by-case basis.

While Appendix A describes each of the financing mechanisms in detail, it is important to first understand the basic workings of public finance, particularly where infrastructure improvements are concerned. To arrive at the appropriate funding strategy, the City will have to make a series of decisions about the implementation process for each of the catalyst projects:

16.2.1 Pay-As-You-Go or Debt Financing

The first step in deciding how to finance a catalyst action or project is to determine whether the appropriate financing strategy is *pay-as-you-go*, or *debt financing*.

- In the *pay-as-you-go* approach, the improvement would only be made once a sufficient amount of tax or fee revenue is gathered to fund the improvement. For example, the City

currently maintains a Parking District that has been collecting revenue for parking improvements over time.

- This contrasts with the *debt financing* approach, where the money for an improvement is borrowed now by issuing bonds, the improvement is made now, and is paid for over time through tax or fee revenue. In 1999, the City used debt financing to fund \$1.2 million of a \$6 million improvement project along parts of Alhambra Creek. The community has benefited from both a reduced risk of flood damage to buildings, and from higher property values in neighboring buildings. Because the assessment district financing structure used in this case is based on property tax revenues, the resulting increased property values from the improvements have reduced the risk associated with debt financing.

	<u>Pros</u>	<u>Cons</u>
<i>Pay-as-you-go</i>	Very little financial risk to City or District	Improvement takes a long time to implement, could be less effective
<i>Debt Financing</i>	Improvement made immediately, could be more effective as a result	Some risk that revenue will not be sufficient to pay off debt within time limit

16.2.2 Benefit Assessment District or Tax Increment Financing

Once the financing method has been identified, the City needs to determine how to gather the additional revenue needed to pay for the catalyst project or action. Two of the more prominent methods of collecting local revenue are the *benefit assessment district* and *tax increment financing*:

- A *benefit assessment district* is formed to include a geographical area in which all property owners would equally benefit from the proposed improvement. Property owners or businesses within the district area would pay an additional tax or fee in the amount necessary to pay for the improvement in the desired time frame. The individual tax or fee would be lower if the district encompassed a large area, or with a long financing time frame.
- *Tax increment financing* is administered at no additional cost to the property owner or individual businesses, by freezing the property tax revenue at its “base rate” in the current year, and diverting any additional tax revenue each year into a separate pool of money used to finance the improvements, as well as state-required pass-throughs, and the 20% set-aside for housing. Tax increment financing is only implemented with the establishment of a redevelopment agency, or an infrastructure financing district.

	<u>Pros</u>	<u>Cons</u>
<i>Benefit Assessment District</i>	<ul style="list-style-type: none"> ▪ Less financial risk to City or public agency; individual property owners take on more risk, although the City may have to take on a defaulted loan under State law. ▪ Could lead to increased tax revenue based on private reinvestment 	<ul style="list-style-type: none"> ▪ Individual property owners may be unwilling to absorb financing risk, especially for debt financing. Therefore, could be ineffective in the short term if approved. ▪ Assessment is considered another form of taxation.
<i>Tax Increment Financing</i>	<ul style="list-style-type: none"> ▪ Improvement does not cost individual property owners additional fees or taxes ▪ Improvements may lead to increases in sales and property tax revenue adjacent to redevelopment area 	<ul style="list-style-type: none"> ▪ Some risk to Redevelopment Agency if incremental tax revenue does not cover financing costs for improvements ▪ Takes future tax revenue that would otherwise go to the general fund; diverts this revenue to debt service for bonds

16.3 IMPLEMENTATION MATRICES

The implementation matrices that follow, Tables 16-1 and 16-2, provide the following information:

- Identify the catalyst projects or actions
- Describe how the catalyst projects or actions benefit the Downtown
- Connect the catalyst projects or actions to the Specific Plan goals and policies that they could help achieve
- Identify a variety of financing options that the City could potentially use to implement the catalyst projects or actions

The catalyst projects/actions fall into three categories, depending on the level of public involvement needed to execute them:

- The first category includes **publicly-funded improvements**, or specific one-time infrastructure costs that require a significant level of City involvement and public financing to complete. The costs of public improvements ultimately will be borne equally by all residents of Martinez or the Downtown area by allocating public funds to pay for these improvements (see Section 16.2 of this chapter, “Financing Mechanisms,” for more information).
- The second category includes **ongoing programs** that can either be administered by a public or private agency. The costs to the City of these catalyst projects vary depending on the financing sources that the City can secure. These projects are often categorized as “public-private partnerships.”

- The third category includes **policy-level changes** that require a low relative cost on the part of the City, but that could potentially stimulate a high level of investment from property owners or developers within the Downtown.

Table 16-1 Description of Catalyst Projects
Downtown Martinez Specific Plan

CATEGORY Catalyst Project/Action	Benefits	Potential Sites	Factors/Requirements
<i>PUBLIC IMPROVEMENTS</i>			
Parking Structure	Improves parking in commercial areas; Helps free up existing surface parking lots for more appealing development; Attracts customers to retail businesses; Encourages new retail businesses to move to Downtown; Encourages visitors downtown to park once and walk around. Depending on the location, could provide parking for Willows Theater.	Sites 13 (Community College parking lot); 24 (SW corner Ward/Las Juntas); 25 (behind Veterans Building); or on a County parking lot	1/2 block or larger site needed. Demand is on east side. Could develop a private parking structure but parking could be relatively expensive unless subsidized. If structure is built near, and shared with intermodal station, it may be eligible for partial financing through Federal Transit Capital Investment Grants.
Additional Creekside Improvements: pedestrian path, children's playground, lighting, new landscaping	Makes the downtown more visually appealing; Provides an attractive and direct route from the Intermodal station to the Downtown commercial areas; Encourages visitors and residents to walk; Improves the connection of the downtown with the waterfront	Along Alhambra Creek, esp. NW corner Green/Ferry	Explore funding options: TLC grants from MTC, state and federal funding may also be available.
Relocation of City Corporation Yard and new (public or private) development on site	Provides a highly accesible site for new development; Since it is already city-owned, it is more readily available for public uses than other sites in the Downtown; Improves the image and quality of life for surrounding residents; Its development could could spur other owners of vacant or industrial sites to consider new development options	Portion of Site 5 (current corp. yard). Proposal to move corp. yard to Water Treatment Plant site	Already identified in Capital Improvements program, but no identified funding source. Need identification of funding to pay for relocation of yard, and cleanup; if new development is public, must identify funding sources for development

Table 16-1 Description of Catalyst Projects
Downtown Martinez Specific Plan

<i>CATEGORY</i> Catalyst Project/Action	<i>Benefits</i>	<i>Potential Sites</i>	<i>Factors/Requirements</i>
<i>PUBLIC IMPROVEMENTS Cont'd</i>			
Improved streetlighting and streetscaping	Encourages visitors and residents to remain in commercial areas after working hours and on weekends; Beautification enhances the look of businesses	Main, Ferry, Court, others, esp. along Alhambra Creek greenway	Could be developed with TLC Capital Program grant; could be supported through the creation of a Business Improvement District (BID)
Improved electricity and gas grid	Distributes costs of utility upgrades among all property owners, thus reducing unequal cost burden on first owners reinvesting in historic buildings; enables owners to attract high end retail tenants and businesses requiring more electricity (such as restaurants)	Downtown commercial area (boundaries should be considered by City and property owners)	
<i>PROGRAMS AND GRANTS</i>			
Contract with business recruitment marketing consultant	Business recruitment consultants bring experience with attracting new tenants, and have connections to tenants looking for new locations. Can also define, in great detail, the size and amenity requirements of potential new retailers	Downtown commercial area	Chamber of commerce, Main Street could be involved in hiring this consultant
Grant program for Unreinforced Masonry (URM) improvements	Preserves the historic nature of the Downtown; may save small businesses that occupy unreinforced buildings; makes the Downtown safer in seismic activity; Enables new businesses to occupy currently vacant buildings; encourages reuse of older buildings; may attract new retail tenants	All sites with URM structures	Identification of sources of funding for grant program; development of an application process; would need to encourage building owners to apply
Façade improvement grant program or small business loan program	No or low interest loans provide property owners and small businesses with the additional equity they need to complete improvements to older buildings, thus making them more attractive to new tenants and patrons alike. Also, city-sponsored loans help qualify land owners for matching funds from federal/state sources.	Older commercial buildings along Main St., Green St., Las Juntas St.	

Table 16-1 Description of Catalyst Projects
Downtown Martinez Specific Plan

CATEGORY Catalyst Project/Action	Benefits	Potential Sites	Factors/Requirements
<i>POLICY CHANGES</i>			
Changes to Land Use Regulations as proposed in Specific Plan	Encourages development of townhomes and condominiums, which are not allowed under current regulations. Takes advantage of a strong residential market; Increases the amount of housing stock in the Downtown; Placement of residential in more commercial areas creates a lively Downtown on the evenings and weekends; Stimulates overall reinvestment and attracts people to the Downtown	Scattered vacant sites; will affect most opportunity sites identified earlier.	Will be established with approval of Specific Plan. Most economically feasible as 3-4 stories with tuck-under parking on ground floor; current zoning forbids townhouses, 3+ story, and housing in commercial zones
Establishment of local historic preservation district	Qualifies buildings within district for property-tax reduction incentives if approved by County, City; Preserves the historic nature of the Downtown; encourages improvement of commercial buildings	Downtown Commercial Area	Will be established with approval of Specific Plan.
Establishment of state and federal historic preservation district	Qualifies buildings within district for financial incentives from Federal government; Preserves the historic nature of the Downtown; prevents the destruction of historic landmarks; encourages improvement of commercial buildings	All buildings qualifying under guidelines for 10 percent and 20 percent tax credit	Would require district-wide evaluation and survey of historic buildings, completed using standards provided by the State. Enables property owners to apply for federal tax credits and other public sources of financing, but application is competitive.
Establishment of property assessment district for infrastructure, utility improvements, Business Improvement District, or other districts	Provides financing for a variety of projects including lighting and street improvements, utility and wastewater drainage upgrades. Projects must equally benefit all property owners in district, and in this case will reduce costly utility upgrade burden on owners interested in improving their properties, could positively impact property value as well.	Commercial area in downtown (to be determined by City, property owners)	Creation of a district requires State, County approval as it changes property taxes. Districts could provide distinct benefits and allow for geographic flexibility (i.e. if residential areas needed wastewater drainage improvements as well, can expand this district to include these areas). Would also need to determine whether financing strategy is pay-as-you-go or debt financing.
Reexamination of way that parking district is structured	Restructuring parking district could make it more effective in improving public parking in the Downtown	Downtown Commercial Area	Parking district has historically underperformed in providing public parking areas. This is in part because parking district provides "pay-as-you-go" financing which takes a long time to accumulate.

Table 16-2 Potential Financing Sources for Catalyst Projects/Actions
Downtown Martinez Specific Plan

CATEGORY	Catalyst Project/Action	Goals and Policies Addressed	Timing: <i>Short Term</i> (Within 2 Years) <i>Mid Term</i> (2 to 5 Years) <i>Long Term</i> (5 to 20 Years)	Costs: <i>Low</i> (Less than \$50,000) <i>Medium</i> (\$50,000 to \$300,000) <i>High</i> (\$300,000 to several million)	Potential Funding Sources																Reference in Specific Plan Text	
					Assessment Districts				Private Investment			Other Government Sources of Funding						Direct City Financing				Description of other
					Creation of Business Improvement District (BID)	Property Based Improvement District (PBID)	Lighting and Landscape Assessment District	Parking Assessment District	Infrastructure Financing District	Maintenance Assessment District	In-lieu Development Fee	Coordination with Chamber of Commerce	Coordination with Main Street	California Mills Act Historic Preservation Agreement	California Heritage Fund Grant	Federal Historic Tax Credits (10 or 20 percent)	Federal Certified Local Government program	Contra Costa County Measure C Discretionary Funds	MTC's TIC: Transportation for Livable Communities program	Establishment of Redevelopment Agency		
PUBLIC IMPROVEMENTS																						
Parking Structure	Policy LU-1-2 Policy LU-1-3 Policy LU-1-4 Policy LU-1-5 Policy LU-1-6 Policy H-1-5 Policy ED-1-3 Goal P-1	Short Term	High (\$950,000 to \$1.2 million)	X	X		X										X	X	X	Federal Transit Capital Investment Grants	Chapter 11	
Additional Creekside Improvements: pedestrian path, children's playground, lighting, new landscaping	Policy LU-1-1 Policy OS-1-2 Goal UD-1 Policy C-1-3 Policy OS-1-2	Mid Term	Medium to High						X	X	X						X	X	X		Section 13.5 Section 13.6	
Relocation of City Corporation Yard and new (public or private) development on site	Policy LU-1-1 Policy LU-1-4 Policy LU-1-5 Goal H-1 Policy ED-1-3	Mid Term	High (est. \$4,000,000) Net cost could be Low														X	X	X	Sale of site could partly cover costs of new site		
Improved streetlighting and streetscaping	Policy LU-1-6 Goal UD-1 Policy C-1-2 Policy C-1-5 Policy C-1-3 Policy C-1-6	Short Term	Medium	X	X	X			X		X	X				X	X				Chapter 14	
Improved electric and gas grid	Policy LU-1-2 Policy LU-1-4 Policy H-1-5 Policy ED-1-2 Goal HP-1	Mid Term	High															X	X		Chapter 15	

Table 16-2 Potential Financing Sources for Catalyst Projects/Actions
Downtown Martinez Specific Plan

Catalyst Project/Action	Goals and Policies Addressed	Timing: <i>Short Term</i> (Within 2 Years) <i>Mid Term</i> (2 to 5 Years) <i>Long Term</i> (5 to 20 Years)	Costs: <i>Low</i> (Less than \$50,000) <i>Medium</i> (\$50,000 to \$300,000) <i>High</i> (\$300,000 to several million)	Potential Funding Sources																Reference in Specific Plan Text	
				Assessment Districts						Private Investment		Other Government Sources of Funding						Direct City Financing			Description of other
				Creation of Business Improvement District (BID)	Property Based Improvement District (PBID)	Lighting and Landscape Assessment District	Parking Assessment District	Infrastructure Financing District	Maintenance Assessment District	In-lieu Development Fee	Coordination with Chamber of Commerce	Coordination with Main Street	California Mills Act Historic Preservation Agreement	California Heritage Fund Grant	Federal Certified Local Government program	Federal Historic Tax Credits (10 or 20 percent)	Contra Costa County Measure C Discretionary Funds	MTC's TIC: Transportation for Livable Communities program	Establishment of Redevelopment Agency		
PROGRAMS AND GRANTS																					
Contract with business recruitment marketing consultant	Policy LU-1-2 Policy LU-1-5 Policy LU-1-6 Policy ED-1-1	Short Term	Low to Medium	X							X	X					X	X			
Grant program for Unreinforced Masonry (URM) improvements	Policy LU-1-1 Policy LU-1-7 Policy H-1-4 Policy H-1-5 Policy ED-1-2 Goal HP-1	Mid Term	High										X	X	X	X	X	X	X	Community Development Block Grant (CDBG)	Chapter 7
Facade improvement grant program or small business loan program	Policy ED-1-1 Policy ED-1-2 Policy UD-1-3 Policy HP-1-2 Policy HP-1-3	Long Term	Medium							X	X	X	X	X	X		X	X	X	Community Development Block Grant (CDBG)	

Table 16-2 Potential Financing Sources for Catalyst Projects/Actions
Downtown Martinez Specific Plan

Catalyst Project/Action	Goals and Policies Addressed	Timing: <i>Short Term</i> (Within 2 Years) <i>Mid Term</i> (2 to 5 Years) <i>Long Term</i> (5 to 20 Years)	Costs: <i>Low</i> (Less than \$50,000) <i>Medium</i> (\$50,000 to \$300,000) <i>High</i> (\$300,000 to several million)	Potential Funding Sources																	Reference in Specific Plan Text		
				Assessment Districts					Private Investment			Other Government Sources of Funding							Direct City Financing			Description of other	
				Creation of Business Improvement District (BID)	Property Based Improvement District (PBID)	Lighting and Landscape Assessment District	Parking Assessment District	Infrastructure Financing District	Maintenance Assessment District	In-lieu Development Fee	Coordination with Chamber of Commerce	Coordination with Main Street	California Mills Act Historic Preservation Agreement	California Heritage Fund Grant	Federal Certified Local Government program	Federal Historic Tax Credits (10 or 20 percent)	Contra Costa County Measure C Discretionary Funds	MTC's TIC: Transportation for Livable Communities program	Establishment of Redevelopment Agency	City General Fund			Other
POLICY CHANGES																							
Changes to Land Use Regulations as proposed in Specific Plan	Goal LU-1 Goal H-1 Policy ED-1-3 Policy UD-1-1 Policy HP-1-3 Policy P-1-1	Short Term	Low to Medium																	X	Throughout		
Establishment of Local Historic Preservation District	Policy H-1-2 Policy H-1-4 Policy ED-1-2 Policy UD-1-5 Goal HP-1	Short Term	Low									X	X						X	X	X	Coordination with Martinez Historical Society	Chapter 7
Establishment of State and Federally-recognized Historic District	Policy LU-1-1 Policy H-1-2 Policy H-1-4 Policy ED-1-2 Policy UD-1-5 Goal HP-1	Mid Term	Medium							X	X								X	X	X	Coordination with Martinez Historical Society	Chapter 7
Establishment of property assessment district for infrastructure, utility improvements, Business Improvement District, or other districts	Policy LU-1-4 Policy ED-1-2 Policy ED-1-4 Policy UD-1-2 Policy UD-1-3 Policy UD-1-4 Policy UD-1-5	Mid Term	Low to Medium																X	X			Chapter 15
Reexamination of way that parking district is structured	Goal P-1	Short Term	Low																X	X			